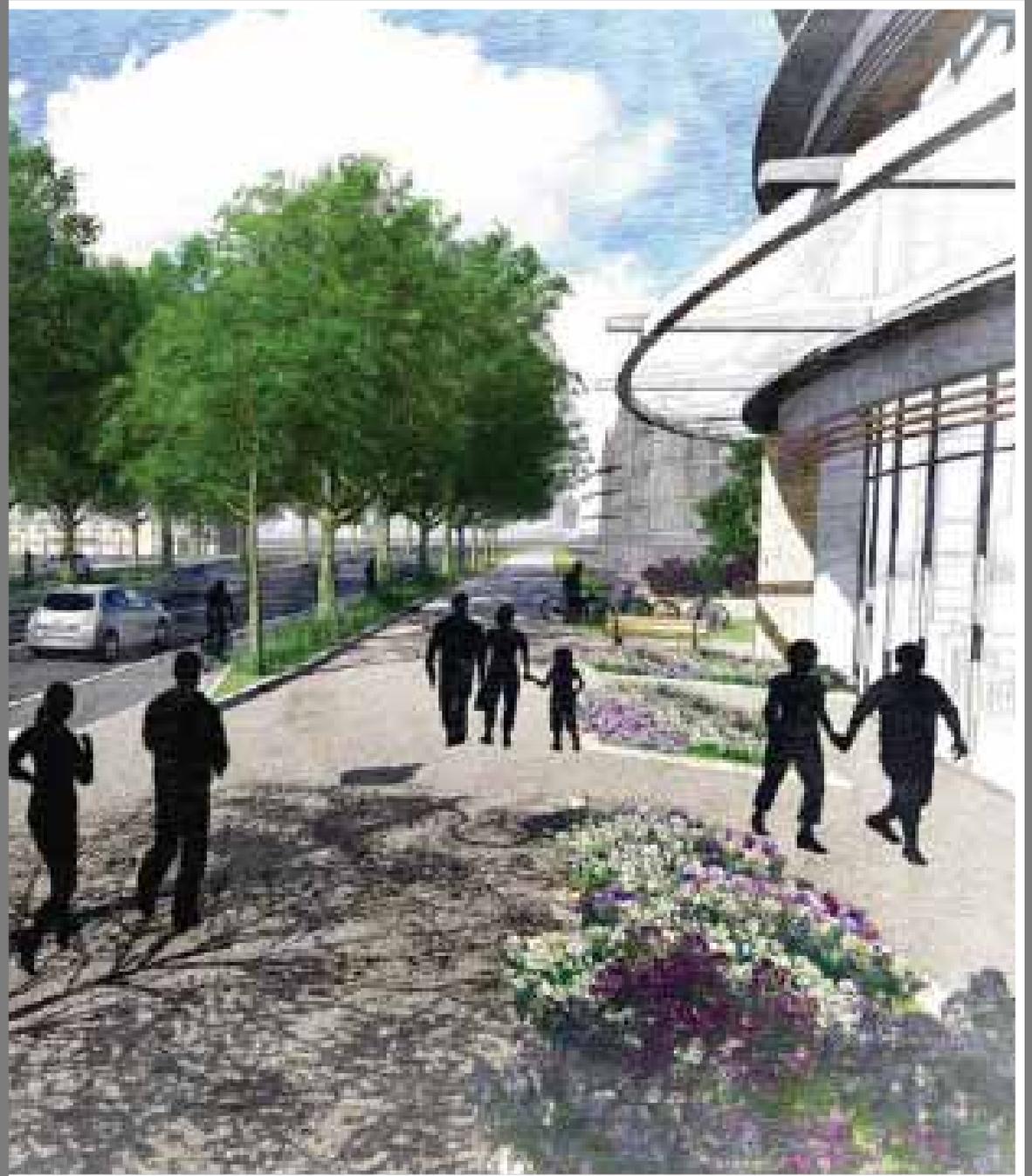
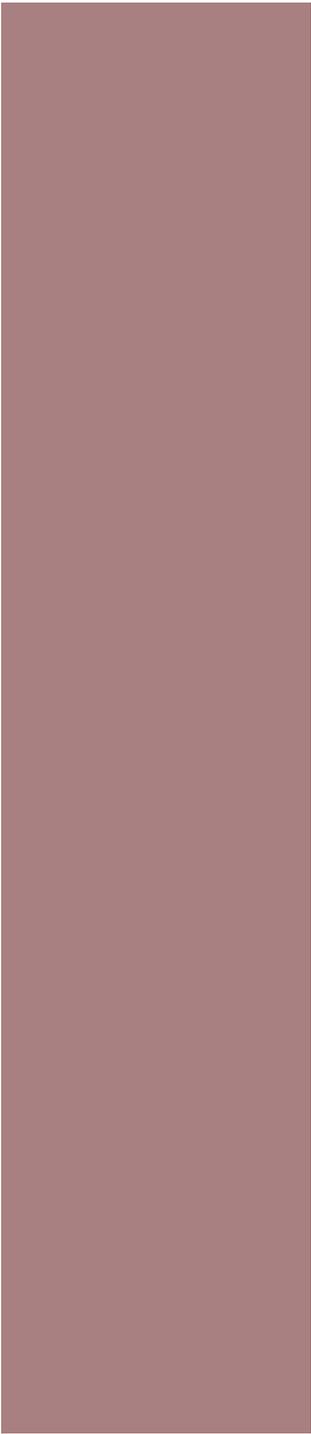


***Brookhaven -  
Oglethorpe  
MARTA Station  
Charrette Report  
Appendix***



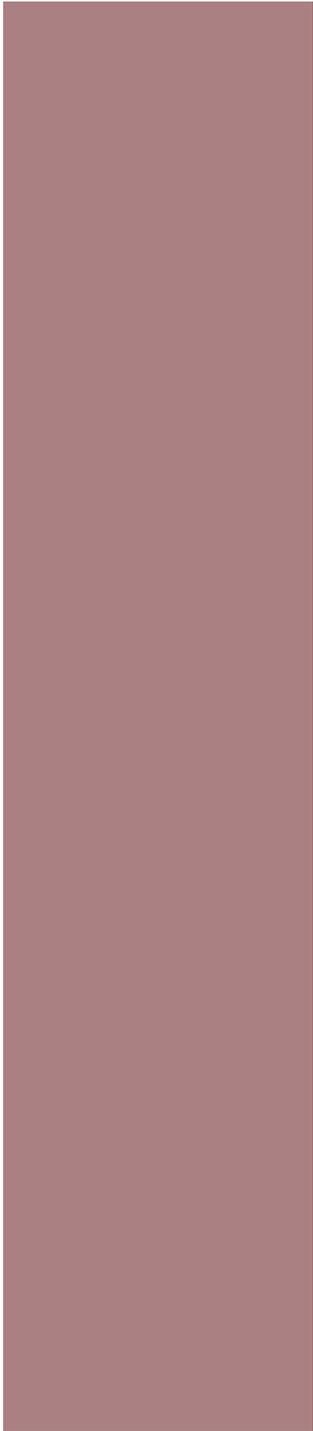


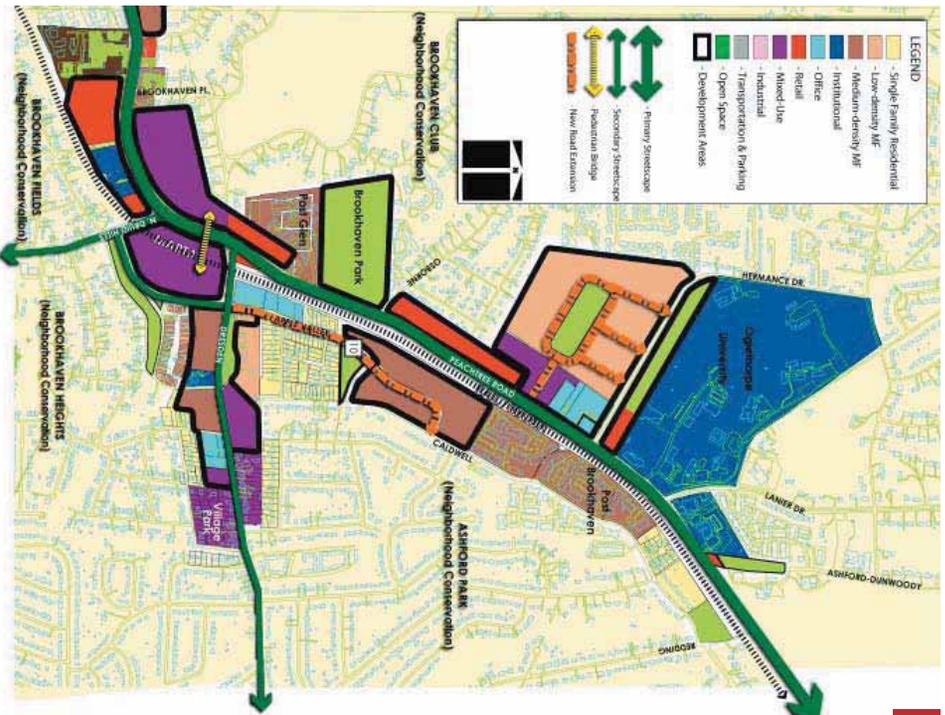


## Appendix

- A. Brookhaven-Peachtree Design Guidelines
- B. Brookhaven-Oglethorpe MARTA Charrette-Summary of Neighborhood Engagement
- C. Stormwater Facility and City Park Full Concept Report, Calculations and Cost Summary
- D. Renderings - Streetscape and Regulating Plans
- E. Background Sessions Presentations (3)
- F. Media Coverage

## Appendix A: Brookhaven-Peachtree Design Guidelines





Brookhaven-Peachtree LCI Framework Plan

**Background:**

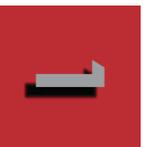
In 2005, the Atlanta Regional Commission granted DeKalb County a planning grant through its Livable Centers Initiative (LCI) to develop a long range plan for the Brookhaven-Peachtree Area including the Brookhaven/Oglethorpe University MARTA station area and the Peachtree Road corridor from the Fulton County line to Ashford Dunwoody Road. The planning process included three public workshops averaging over 100 participants each and monthly meetings with a stakeholder core team to enlist the community's help in developing a plan for future development and redevelopment in the Brookhaven-Peachtree Area. A Framework Plan for the Core LCI Study Area was created outlining future land uses, potential transportation improvements and desired open space improvements. Once the general Framework Plan was established, more detailed site development concepts were generated for priority areas to show potential redevelopment of major sites in accordance with the community's vision.

A set of urban design and development guidelines were established as part of the Brookhaven-Peachtree LCI Plan to encourage development and redevelopment that is consistent with the LCI vision. These guidelines were used as the basis for the Brookhaven-Peachtree Overlay District, an amendment to the DeKalb County zoning ordinance, which is described within this document. This ordinance will serve as a legal, enforceable code to implement the LCI plan.



Brookhaven-Peachtree LCI Illustrative Plan @ the Brookhaven-Oglethorpe MARTA Station

**BROOKHAVEN - PEACHTREE DESIGN GUIDELINES**



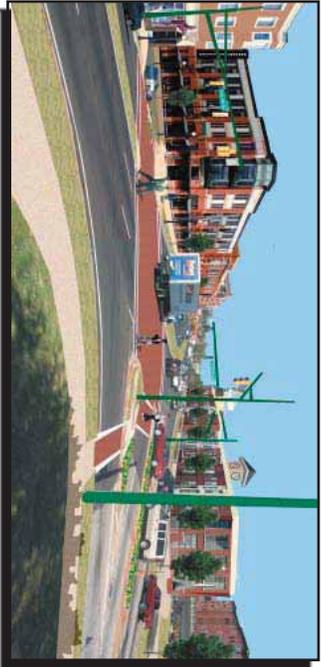
**LCI PLAN**

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March 2007



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Prepared For: Brookhaven - Peachtree LCI



**Future Peachtree Road @ North Druid Hills Simulation\***

**Purpose and Intent:**

- To promote the development of a more dynamic, mixed-use urban village of appropriate scale and magnitude surrounding the Brookhaven-Oglethorpe University MARTA transit station;

- To ensure that new structures and developments are consistent with the vision and recommendations of the Brookhaven-Peachtree LCI Plan;

- To provide for a variety of housing types and promote mixed-income residential opportunities;

- To facilitate access to the Brookhaven-Oglethorpe University MARTA transit station and to compatible and neighboring commercial, civic, recreational and institutional uses and residential areas;

- To design and arrange structures, buildings, streets and open spaces to create an inviting, walkable, mid-rise, human-scale environment;

- To provide for the efficient and effective use of land surrounding the Brookhaven-Oglethorpe University MARTA transit station and along Peachtree Road;

- To provide for connectivity of streets and communities and reduce the dependence on automobile use by increasing the ease of movement and opportunities for alternative modes of travel; and

- To ensure a proportional relationship of surrounding buildings with respect to the general spacing of structures, building mass and scale, and street frontage by using techniques to achieve compatibility, such as:

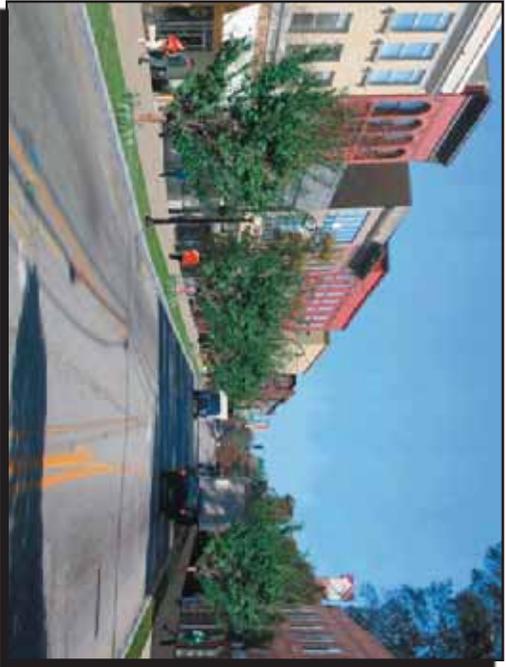
- Use of additional facade detail, proportion of facade elements, doorways, projections and insets; window scale and pattern; and creation of strong shadow lines as decorative elements;

- Use of consistent setbacks from property lines;

- Development of consistent sidewalks and a more active, interesting pedestrian environment;

- Use of landscaping, lighting and street furniture to unify district buildings and define space; and

- Use of compatible building materials to promote a design and building aesthetic consistent with Oglethorpe University and the desired urban village character.



**Future Dresden Drive West of Elijah Drive Simulation\***



**Future Cherokee Plaza on Peachtree Road Simulation\***

**\*Simulations taken from 2005 Brookhaven-Peachtree LCI Plan are illustrative of the desired scale & character of development/redevelopment projects within the Brookhaven-Peachtree Overlay District.**

# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES

## 2

## PURPOSE & INTENT

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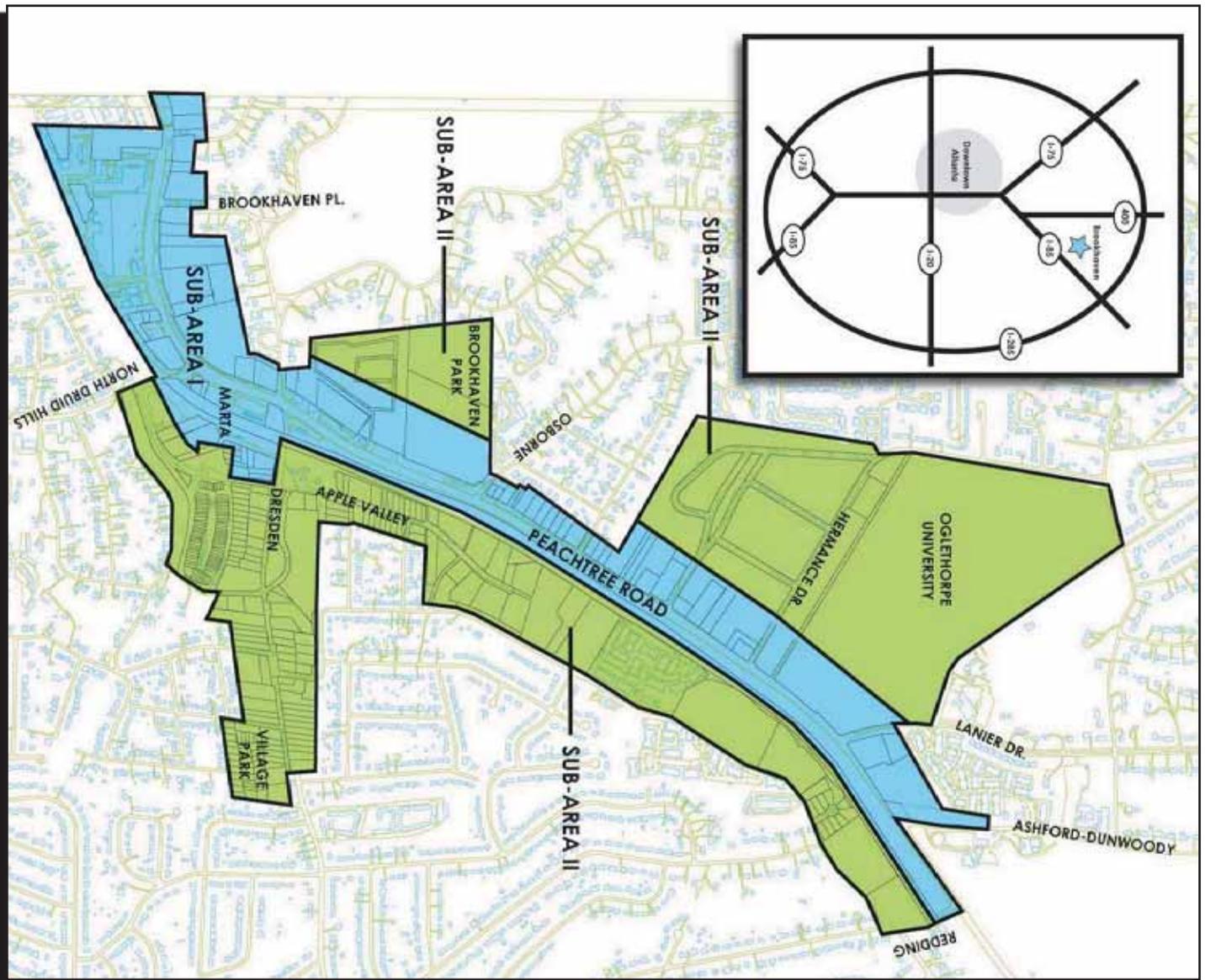
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**District Boundaries:**

The design and development standards within this document apply to any development that lies, in whole or in part, within the Brookhaven-Peachtree Overlay District boundary as illustrated and detailed in the Brookhaven-Peachtree Overlay District. Wherever the underlying zoning regulations are in conflict with the provisions of this overlay district, the overlay provisions shall apply.



**3**

**DISTRICT  
BOUNDARIES**

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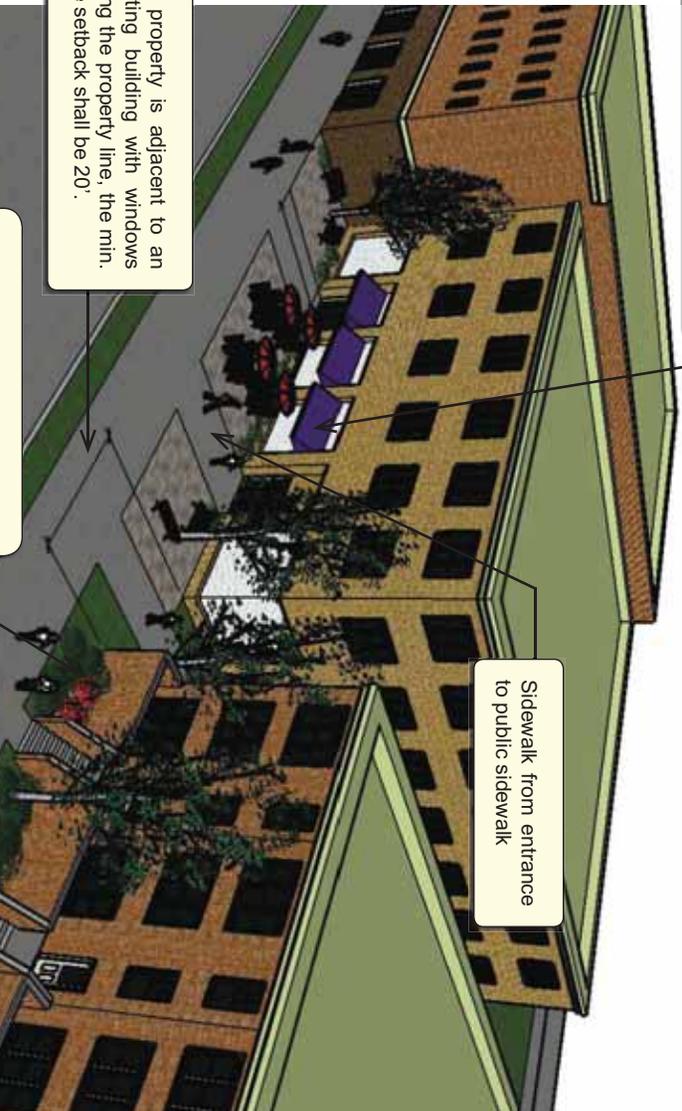


**BROOKHAVEN - PEACHTREE DESIGN GUIDELINES**

**Guidelines:**

- Minimum front setback shall be zero (0) feet. Buildings shall be permitted to be setback from the public sidewalk to allow for building stoops, front porches, balconies or steps, a public space or public park, outdoor dining, outdoor merchandising, or landscaping that does not completely cover the facade of the building. All ground floor uses shall have a sidewalk or clear hardscaped pathway connecting the entrance to the public sidewalk along the street. Parking located between a building and a primary or secondary street (Peachtree Road, Apple Valley Road, Dresden Drive, North Druid Hills Road, Herrance Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Road, or Brookhaven Place) shall be prohibited.

- The minimum side and rear setbacks shall be zero (0) feet, however if the property is adjacent to an existing building with windows facing the property line, the setback shall be no less than twenty (20) feet from the face of the existing building. The side of a building facing a public street shall be permitted to setback from a public sidewalk to allow for building stoops, front porches, balconies or steps, a public plaza or park, outdoor dining, outdoor merchandising, or landscaping that does not completely cover the facade of the building. All ground floor uses must have a sidewalk or clear hardscaped pathway connecting the entrance to the public sidewalk along the street.



Buildings may setback to allow for public space, parks, or outdoor dining.

If a property is adjacent to an existing building with windows facing the property line, the min. side setback shall be 20'.

Buildings may setback to allow for stoops, porches, balconies, steps, or landscaping.

Sidewalk from entrance to public sidewalk

# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES

## 4

### SETBACKS

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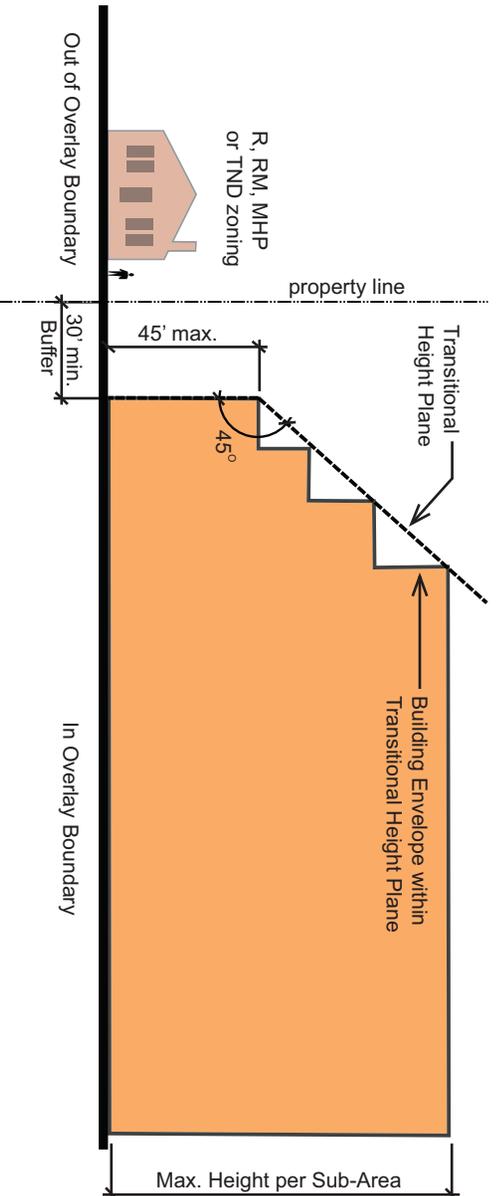
March 2007



**Guidelines:**

- Where a lot on the external boundary of the Brookhaven-Peachtree Overlay District adjoins the boundary of any property outside the district that is zoned for any R-zoning classification, RM-zoning classification, MHP-zoning classification, or TND-zoning classification, a transitional buffer of not less than thirty (30) feet in width shall be provided and maintained in a natural state or so as to maintain an effective visual screen. Said transitional buffer zone shall not be paved or otherwise covered with impervious surfaces and shall not be used for parking, loading, storage or any other use, except that portions of the transitional buffer zone may be utilized for installation of utilities when necessitated by the development, and when the applicant shows that the utilities cannot be located outside of the transitional buffer zone. Water detention ponds shall not be located within the transitional buffer zones. No trees, other than dead or diseased trees, shall be removed from said transitional buffer zone, but additional trees and plant material may be added to the transitional buffer zone.

- Where a lot on the external boundary of the Brookhaven-Peachtree Overlay District adjoins the boundary of any property outside the district that is zoned for any R-zoning classification, RM-zoning classification, MHP-zoning classification, or TND-zoning classification, a transitional height plane shall apply. No portion of any structure within the Brookhaven-Peachtree Overlay District shall protrude through such transitional height plane beginning forty-five (45) feet above the property line between the district and the adjacent property outside the district and extending parallel to the ground inward thirty (30) feet over the transitional buffer zone and then at an upward angle of forty-five (45) degrees over the Brookhaven-Peachtree Overlay District.



# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES

## 5

### TRANSITIONAL BUFFERS & HEIGHTS

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**Guidelines:**

- Building materials shall include brick, stone, stucco, wood, and fiber cement siding. Building facades that are visible from Primary roadways (Peachtree Road, Apple Valley Road) and Secondary roadways (Dresden Drive, North Druid Hills Road, Hermance Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Drive, and Brookhaven Place) should predominantly use brick and stone. The following building materials shall be prohibited: vinyl siding, exposed concrete block or concrete masonry units, exposed plywood, metal siding, plastic or PVC, and synthetic stucco or EIFS.



- Buildings of five (5) or more stories shall architecturally differentiate the first one (1) or two (2) stories from those above through the utilization of cornice lines, fenestration and other forms of architectural detailing.



- Large edifices, longer than one hundred (100) continuous feet, shall vary the cornice height and create variations in facades with changes in facade depth, materials, textures, colors, and/or window and door patterns to provide visual interest and prevent a massive imposing appearance.

- Building facades should promote architectural and visual interest. A minimum of 35% of any facade facing a public street shall consist of openings including windows and doorways.

- Service areas, trash dumpsters, trash compaction and all other mechanical areas and equipment shall be screened from view from all streets and public rights of way. Screening shall be permitted to include landscaping, or architectural treatment or color similar to the building to screen the equipment.

- Roof structures visible from the public right of way may be sloped or flat. Sloped roofs shall be hip roofs of less than forty-five (45) degrees in pitch or gable roofs of thirty (30) degrees or greater in pitch. Sloped roofs shall have overhangs of at least 12 inches on all sides and shall not exceed one hundred (100) feet in length without a change in plane. Permitted roofing materials for sloped roofs include asphalt or fiberglass shingles, solar shingles used with asphalt or fiberglass shingles, standing seam metal roof, ceramic tile, slate or synthetic slate. Flat roofs shall have decorative and/or corbelled parapets on all sides visible from the public right of way and drain to internal roof drains and/or to the rear of the structure, limiting downspouts on facades along primary and secondary streets. Simple barrel vaults or roofs are allowable. Mansard, gambrel, and shed roof forms are prohibited.

**Example Images from 2005 Brookhaven-Peachtree LCI Plan Indicative of Desired Development Scale & Character within the Brookhaven-Peachtree Overlay District**

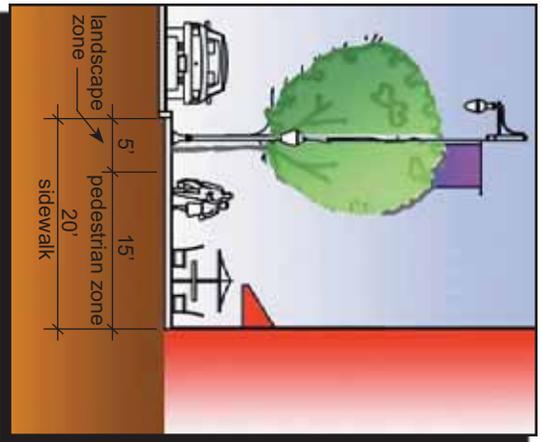
# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES



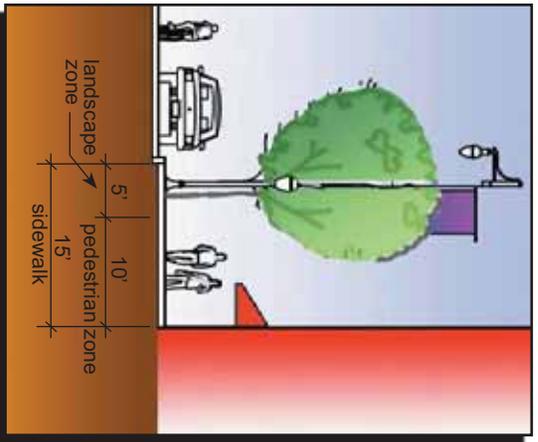
**Streets and Sidewalks:**

- Any new streets shall enhance connectivity within the Peachtree-Brookhaven Overlay District by connecting at least two (2) public streets.
- Sidewalks shall be provided on all streets and shall consist of a landscape zone and a pedestrian zone of widths as provided below.

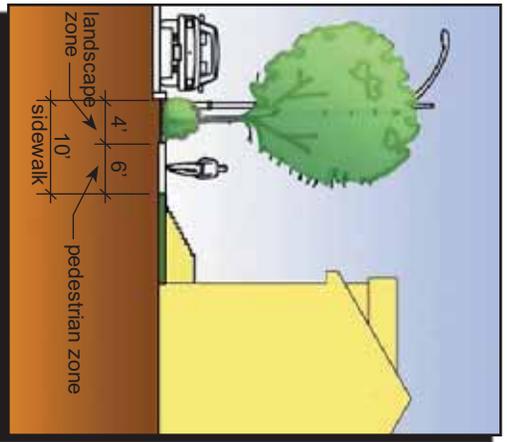
Sidewalk Requirements		
Street	Landscape Zone	Pedestrian Zone
Peachtree	5'	15'
Dresden, N. Druid Hills & Apple Valley (between N. Druid Hills & Sunland)	5'	10'
All Others	4'	6'



**Streetscape 1: Peachtree Road**



**Streetscape 2: Dresden Dr., North Druid Hills Rd, and Apple Valley between North Druid Hills & Sunland Dr.**

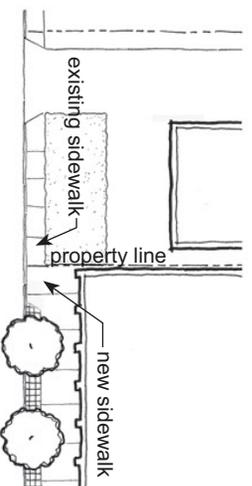


**Streetscape 3: All other new and existing streets within the Brookhaven-Peachtree Overlay District**

- Landscape zones shall be planted with grass, ground cover or flowering plants, or consist of brick pavers, concrete pavers or granite pavers where on-street parking is provided or pedestrian crossings and/or congregation is likely. Pedestrian zones shall be paved in concrete and kept clear and unobstructed for the safe and convenient use of pedestrians.

- Pedestrian sidewalk areas paved with materials other than concrete that are consistent in color with concrete sidewalks may be allowed with the approval of the Director of Planning.

- Where newly constructed sidewalks abut existing adjacent sidewalks, the newly constructed sidewalk shall provide safe facilitation of pedestrian traffic flow to adjacent sidewalks. Any development that disturbs existing sidewalks on an adjacent property shall replace disturbed areas to their pre-disturbance state and condition.



- Safe and convenient pedestrian pathways shall be provided from sidewalks along streets to each structure entrance, including pedestrian access routes to parking decks and through parking lots and between adjacent buildings within the same development. All walkways shall be concrete and a minimum width of five (5) feet.

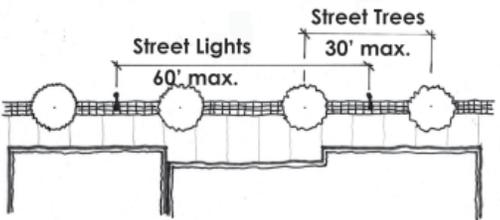
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**SIDEWALKS & LANDSCAPE ZONE**

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**Antique Street Lighting Compatible with Oglethorpe University Lighting**

**Landscape Zone:**

- Street trees shall be planted in all landscape zones spaced at a maximum distance of thirty (30) feet on center.
- New street trees must be a minimum of three and one half (3.5) inches in caliper measured twelve (12) inches above ground, shall be a minimum of sixteen (16) feet in height, shall have a minimum mature height of thirty (30) feet and shall be limbed up to a minimum height of eight (8) feet.

- Street trees shall have a minimum planting area of four feet by eight feet (4' x 8'). Tree planting areas shall provide porous drainage systems that allow for drainage of the planting area.

- Street tree species shall be consistent for an entire block length. Similar species shall be permitted to change on individual block faces only when approved by the Director of Planning with consolidation.

- All initial plantings and plant removal other than routine maintenance or replacement shall be approved by the Director of Planning.

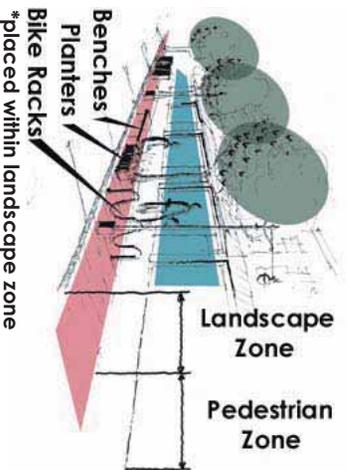
- All sidewalk paving materials shall be continued across any intervening driveway at the same prevailing grade and cross slope as on the adjacent pedestrian sidewalk area.

- Street lights shall be located within the landscape zone spaced at a maximum distance of sixty (60) feet on center on the following streets: Peachtree Road, Dresden Drive, North Druid Hills Road and Apple Valley Road.

- Benches, trash receptacles, and bike racks shall be placed within the landscape zone at high pedestrian traffic areas on the following streets: Peachtree Road, Dresden Drive, North Druid Hills Road and Apple Valley Road.



**Sidewalk paving material shall be continued across driveways.**



**Bench**

**Bicycle Rack**



**Trash Receptacle**



Parking Ratios		
Building Use	Vehicular Parking (minimum and maximum)	Bicycle Parking (minimum)*
Single-Family Attached	1-2 space per dwelling	none required
Multi-Family	1-2 space per dwelling	1 space / 5 units
Commercial	1 space / 400 gross s.f.	1 space / 20 vehicular spaces
Retail	1 space / 300 gross s.f.	1 space / 20 vehicular spaces

\* No development shall have fewer than 3 bicycle spaces nor be required to exceed a maximum of 50 bicycle spaces.

### Parking Requirements:

- Shared parking is encouraged and may result in permitted reductions of off-street parking requirements. Parking facilities within the parcel may be shared if multiple uses cooperatively establish and operate parking facilities and if these uses generate parking demands primarily when the remaining uses are not in operation, so that the above stated off-street parking requirements for each use are met during said use's operational hours. Applicants shall make an application to the Director of Planning for authorization for shared parking. Applicants shall include proof of a written formal shared-parking agreement between all applicants. Required parking for residential units shall be prohibited from being shared.

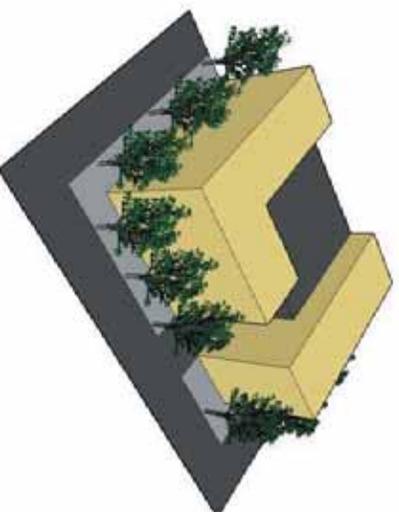
- Required residential parking shall be segregated from parking for all other uses with the exception of additional parking provided for live/work single-family units.

- If required automobile parking spaces cannot be reasonably provided on the same lot on which the principal use is conducted, such spaces may be provided on adjacent or nearby property within the overlay district, provide a majority of such spaces lie within seven hundred (700) feet of the main entrance to the principal use for which the parking is provided.

- On-street parking spaces provided by a development shall be permitted to be counted toward the minimum parking requirements for the respective development for all uses except single-family attached dwellings. On-street parking spaces shall be prohibited from being counted toward the maximum parking requirements.

### Location of Parking:

- All off-street parking including surface lots and parking decks shall be located behind or beside buildings.



### Parking Deck Design:

- Any portion of a parking deck that is adjacent to a public street shall be screened with retail on the ground-floor. Any upper stories of a parking deck that are visible from a public street shall be clad with materials to resemble office or residential buildings with fenestration.



## 9

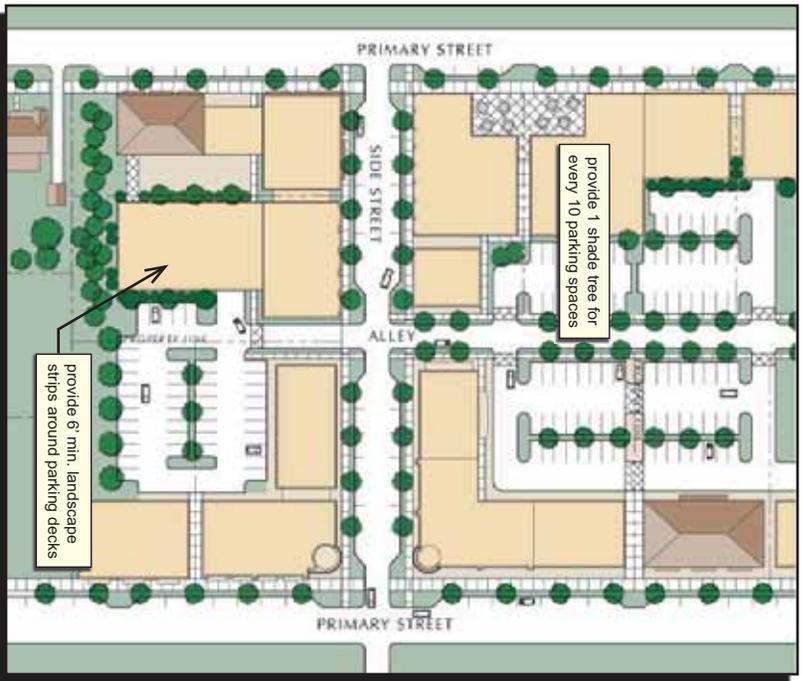
# PARKING REQUIREMENTS

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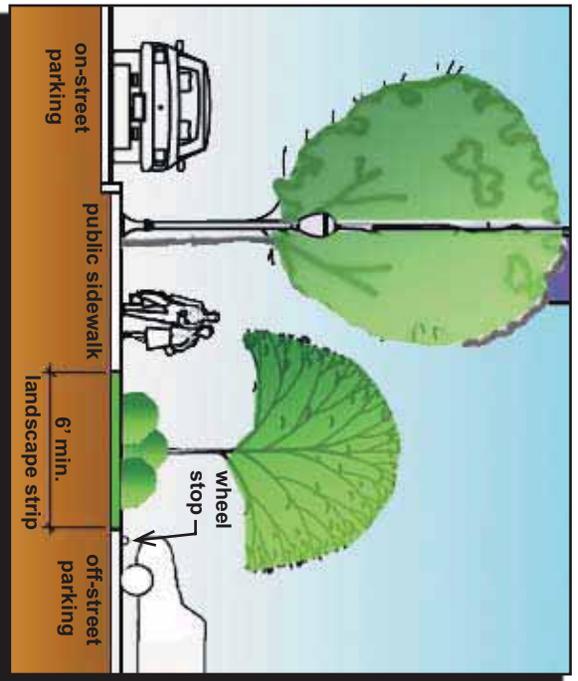


**Screening of Parking:**

- Off-street parking shall be screened from view from any public street using buildings and/or landscaping. Off-street parking may not be located between the public street and the building's front facade. If a parcel is bound by more than one street, the front of the parcel shall be considered the side adjacent to the street with the highest classification as follows: Primary: Peachtree Road and Apple Valley Road; Secondary: Dresden Drive, North Druid Hills Road, Hermance Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Drive, Brookhaven Place; Tertiary: new streets and other existing streets.

**Screening of Parking Lots and Parking Decks**

- Surface parking provided to the side of any building along a public street shall be designated for short-term (no longer than 2 hours) parking and must be buffered from the public street and sidewalk with a landscape strip no less than six (6) feet in width containing a minimum of ninety percent (90%) living shrubs, groundcover, sod and/or annual or perennial color in the landscape strip surface area.
- Wheel stops or bumpers shall be placed at the head of all parking spaces that abut a landscape strip or sidewalk.



**Parking to the Side of a Building Along a Public Street**

- All surface parking lots of 20 parking spaces or more must include landscaping in the form of shade trees within the confines of the surface parking lot. One shade tree must be provided for every 10 parking spaces. Shade trees must be a minimum of three and one half (3.5) inches in caliber measured twelve (12) inches above ground, shall be a minimum of sixteen (16) feet in height, shall have a minimum mature height of thirty (30) feet and shall be limbed up to a minimum height of eight (8) feet.

- All parking decks and parking structures shall have a landscape strip a minimum of six (6) feet in width immediately contiguous to the parking facility for the whole of the exterior perimeter of the parking facility except at vehicular or pedestrian entrances and exits. Such landscape strips shall contain no less than one (1) understory or overstory tree per fifty (50) linear feet, ten (10) shrubs per fifty (50) linear feet, and a minimum of ninety percent (90%) living groundcover, sod and/or annual or perennial color in the landscape strip surface area.

**10 PARKING DESIGN**

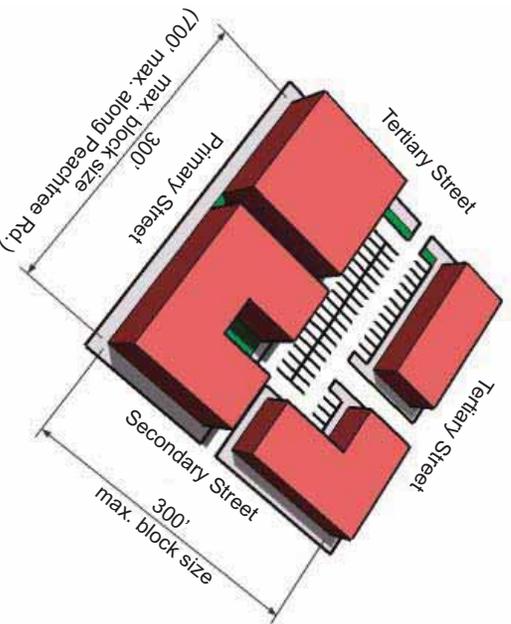
**BROOKHAVEN - PEACHTREE DESIGN GUIDELINES**

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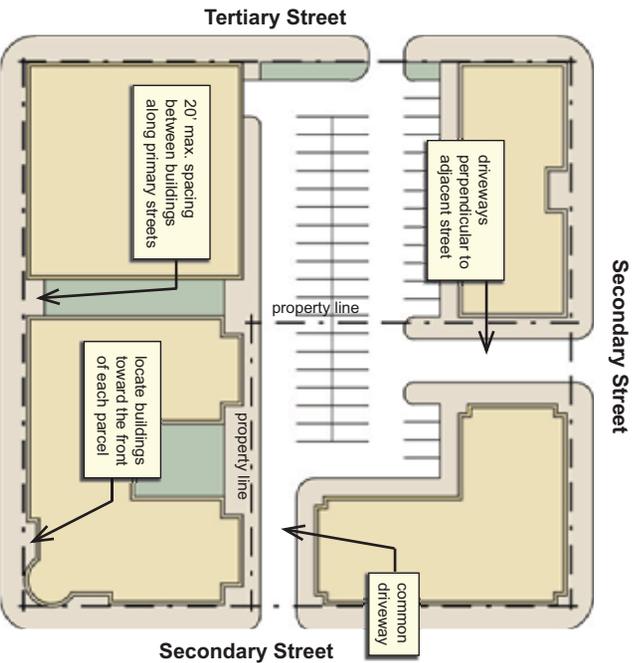
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**Location of Curb Cuts and Allowable Block Sizes**



**Building Locations**

**Guidelines:**

- When blocks are subdivided by new streets, the maximum length of resulting new blocks shall be three-hundred (300) linear feet. Along Peachtree Road, the maximum block length shall be seven-hundred (700) feet.
- The maximum curb radius at any intersection or curb cut is twenty (20) feet.
- All parking shall be accessed via shared alleys or private drives. No curb cuts shall be allowed on primary streets (Peachtree Road and Apple Valley Road) if the development is accessible by another street. If an existing block face on a primary street exceeds seven-hundred (700) linear feet, then one (1) curb cut per seven-hundred (700) linear feet is allowed. If a development is accessible from more than one (1) secondary or tertiary street (Dresden Drive, North Druid Hills Road, Hermance Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Road and Brookhaven Place), the development shall be limited to one (1) curb cut per block face. If a development is only accessible from one (1) street, the development is limited to two (2) curb cuts on that street.
- All curb cuts shall be a maximum of twenty-four (24) feet wide.
- Common or joint driveways are encouraged and may be authorized by the Director of Planning.
- Driveways must be perpendicular to the adjacent street. Circular and non-perpendicular driveways are prohibited.
- Buildings shall be located toward the front of each parcel no more than twenty (20) feet from the public sidewalk. If a parcel is bound by more than one street, the front of the parcel shall be considered the side adjacent to the street with the highest classification as follows:
  - Primary: Peachtree Road and Apple Valley Road;
  - Secondary: Dresden Drive, North Druid Hills Road, Hermance Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Drive, Brookhaven Place, Tertiary: new streets and other existing streets.
- Maximum spacing between buildings along a primary street shall be twenty (20) feet unless a public space, such as a park or plaza, is provided between the respective buildings.

**BROOKHAVEN - PEACHTREE DESIGN GUIDELINES**



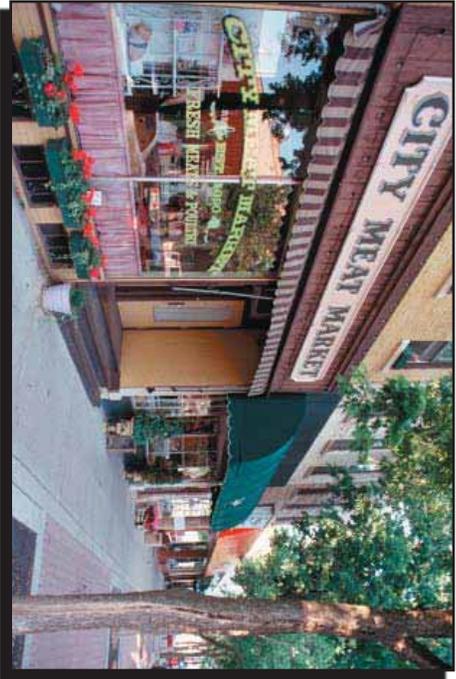
**SITE DESIGN**

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March 2007

Prepared By: Urban Collage, Inc.  
Prepared For: Brookhaven - Peachtree LCI





The primary entrance to all buildings shall be clearly visible from the street, shall face the street and shall be unlocked during operating business hours for all non-residential uses.



Service areas shall be screened from view from the street with buildings, landscaping or decorative fencing.

#### Guidelines:

- The primary entrance shall face the street with the highest classification as follows: Primary: Peachtree Road and Apple Valley Road; Secondary: Dresden Drive, North Druid Hills Road, Hermonce Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Drive, Brookhaven Place; Tertiary: new streets and other existing streets.
- Where development is on a primary or secondary street and also across the street from existing single-family homes, which face the street, the development shall contain buildings facing the primary or secondary street and the existing single-family development.
- All service areas shall be screened from view from the street with buildings, landscaping or decorative fencing.
- Any decorative fencing shall be comprised of brick, stone, wrought iron, or wood. Fencing in the front yard of any property shall not be higher than four (4) feet. Fencing to the rear or side of a building shall not be higher than six (6) feet.
- No barbed wire, razor wire, chain link fence or similar elements shall be visible from any public plaza, ground level or sidewalk level outdoor dining area or public right-of-way.
- Gates and security arms shall be prohibited from crossing any public street or sidewalk.
- All utilities shall be located underground.

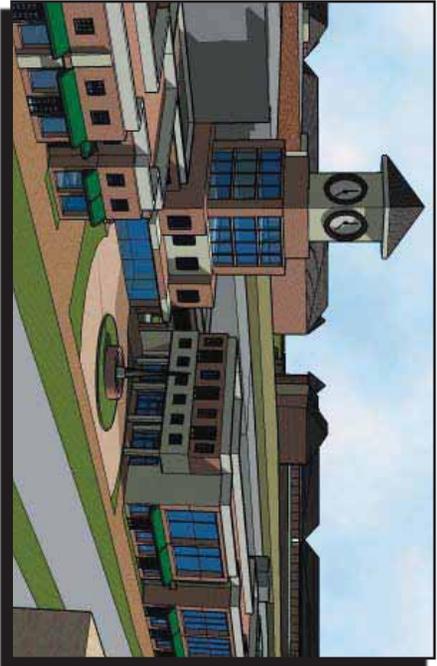
# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES

## 12 SITE DESIGN

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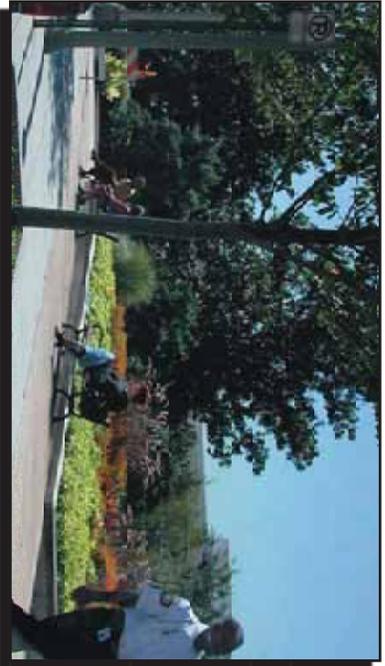
March 2007





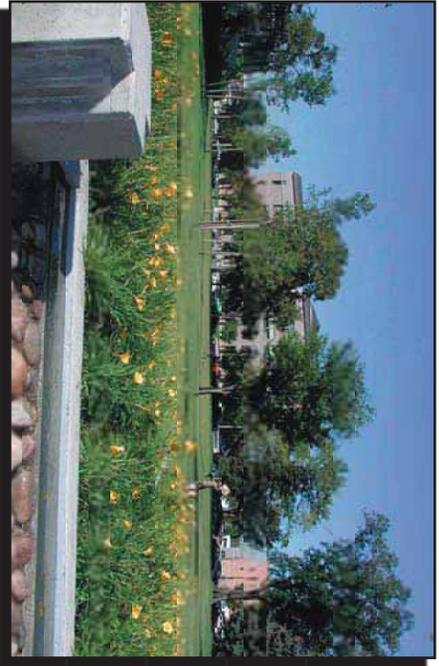
**Public spaces should be surrounded with a mix of ground-floor active uses and be accessible from the public sidewalk.**

- A publicly accessible open space, which is at grade and a minimum of twenty percent (20%) of the total development parcel, shall be provided by each new development that includes residential uses.
- All publicly accessible open spaces shall be at grade, surrounded by buildings with a mix of active uses on the ground floor on at least one side, face the public street, and be directly accessible from a public sidewalk along a street.



**Hardscape**

- Publicly accessible open spaces including front yards, planted areas, fountains, parks, plazas, trails and paths, hardscape elements related to sidewalks and plazas, and similar features which are located on private property and accessible to the general public; on-street parking or the portion thereof located on private property; and natural stream buffers shall be permitted to be counted toward the twenty percent (20%) open space requirement.



**Open Space/Park**

- Private courtyards and other private outdoor amenities shall be located at the interior of the block, behind buildings or on rooftops. Private courtyards and outdoor amenities shall be prohibited from being counted toward the twenty percent (20%) requirement.
- Open Space Implementation. All open space including buffers, setbacks, sidewalk clear zones, sidewalk zones and open spaces shall be fully implemented prior to issuance of a Certificate of Occupancy for the primary development.
- Open Space Maintenance. The owner shall provide adequate maintenance of the open space improvements for a minimum of one (1) year from the date of issuance of the Certificate of Occupancy. When a private property owner provides landscaping within the public right-of-way and the landscaping dies within a one-year period, such landscaping shall be replaced within a reasonable time for planting by the owner at the owner's sole expense.

# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES

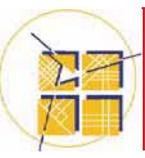
## Guidelines:

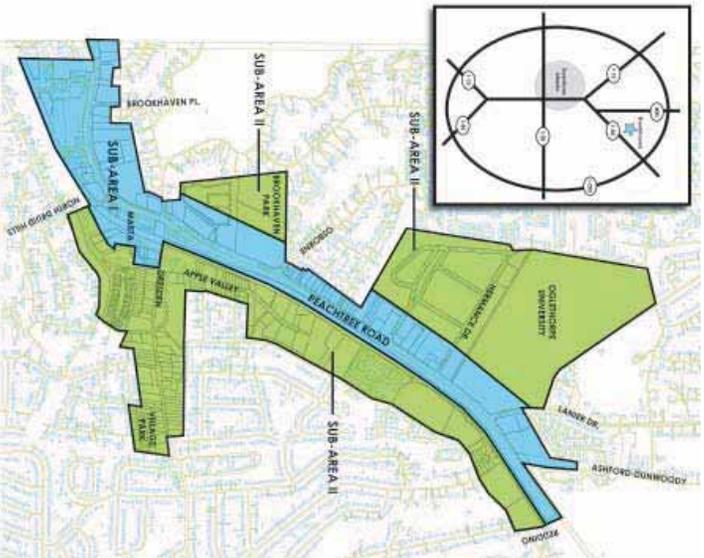
# 13 OPEN SPACE

DRAFT

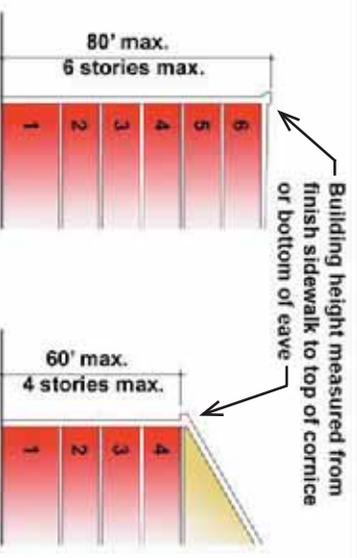
March 2007

Prepared By: Urban Collage, Inc.  
Prepared For: Brookhaven - Peachtree LCI





Map of Sub-Areas



Sub-Area I

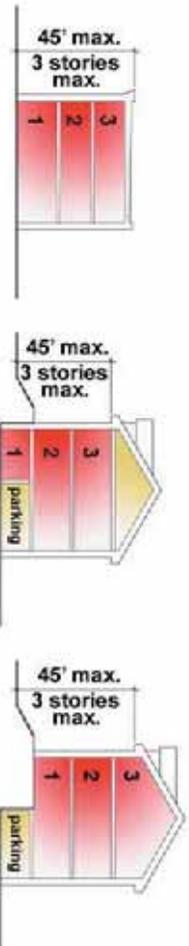
Sub-Area II

**Building Heights:**

- Within the Brookhaven-Peachtree Overlay District, there shall be two sub-areas for the purpose of establishing allowable building heights. Sub-area I shall include the area directly surrounding the Brookhaven MARTA station and parcels with frontage along Peachtree Road up to a maximum distance of four hundred forty (440) feet from the Peachtree Road right of way, where the highest intensity development is desired. Sub-area II shall include the remaining area within the Brookhaven-Peachtree Overlay District, including Dresden Drive, North Druid Hills Road and those areas that serve as a transitional edge between high density development surrounding the MARTA transit station and existing single-family neighborhoods. The boundaries of these sub-areas shall be established by a zoning map amendment adopted pursuant to this chapter which amendment shall be incorporated herein and made a part of this Chapter 27.

- All building heights are to be measured from the finished sidewalk to the top of cornice for flat-roofed buildings or the bottom of the eave for sloped roofed buildings along the facade that faces the street with the highest volume of daily traffic. Levels measuring less than six feet in height for the purposes of raising the first floor level above the street level, providing a consistent first floor level, or for the sole purpose of housing mechanical, plumbing or electrical equipment shall not be counted as stories for the purposes of maximum building heights.
- All buildings within the Peachtree-Brookhaven Overlay District shall be a minimum of two (2) stories tall and twenty-eight (28) feet in height.
- The maximum building height for buildings in Sub-area I shall be six (6) stories or eighty (80) feet whichever is less, and the maximum building height for buildings in Sub-area II shall be four (4) stories or sixty (60) feet, whichever is less.
- Single-family attached townhomes or live/work units in Sub-area I or Sub-area II shall not exceed three (3) stories or thirty-five (35) feet, whichever is less.

**Single-family Attached Townhomes and Live/Work Units**

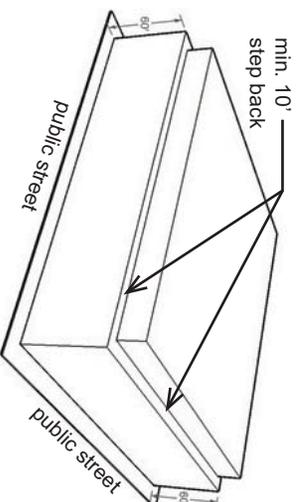


### Building Height Bonus:

A building in the Peachtree-Brookhaven Overlay District shall be permitted to exceed the maximum building height limit provided it meets one or more of the following:

- Single-family attached townhome and live/work units are permitted to add one (1) additional story if twenty percent (20%) of the total number of residential units within a development of fifteen (15) units or more are priced for workforce housing.
- Mixed-use and multi-family buildings are permitted to add one (1) additional story to all buildings within the development if twenty percent (20%) of the total number of residential units within a development of twenty (20) units or more are priced for workforce housing.
- Commercial and residential buildings are permitted to add one (1) additional story to each building on a given parcel if a contiguous, publicly accessible open space as defined by this district, not including on-street parking and public sidewalks along roadways, comprising at least 25% of the development parcel is provided.
- Commercial and multi-family buildings fronting Peachtree Road or Apple Valley Road are permitted to add one (1) additional story to the respective building if seventy-five percent (75%) or more of the ground floor space is used for retail or service commercial purposes.

- Buildings in excess of sixty (60) feet in height in Sub-area II shall be required to step back that portion of the building greater than sixty (60) feet in height a minimum linear distance of ten (10) feet away from the building facade facing any public street.



- The maximum building height with any combination of height bonuses listed above shall be dependent upon building use according to the chart below. Special land use permits and variances shall not alter or allow structures taller than the maximum building heights provided herein within the Brookhaven-Peachtree Overlay District boundary.

Sub-Area I		
Building Use	Max. Building Height	Max. Building Height with Bonuses
Single-Family Attached	3 stories or 45 feet*	4 stories or 55 feet*
Commercial	6 stories or 80 feet*	7 stories or 90 feet*
Multi-family	6 stories or 80 feet*	8 stories or 100 feet*
Mixed-Use	6 stories or 80 feet*	8 stories or 100 feet*

Sub-Area II		
Building Use	Max. Building Height	Max. Building Height with Bonuses
Single-Family Attached	3 stories or 45 feet*	4 stories or 55 feet*
Commercial	4 stories or 60 feet*	5 stories or 70 feet*
Multi-family	4 stories or 60 feet*	6 stories or 80 feet*
Mixed-Use	4 stories or 60 feet*	6 stories or 80 feet*

\* whichever is less

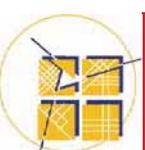
# 15 BUILDING HEIGHTS

# BROOKHAVEN - PEACHTREE DESIGN GUIDELINES

Prepared By: Urban Collage, Inc.  
Prepared For: Brookhaven - Peachtree LCI

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March 2007





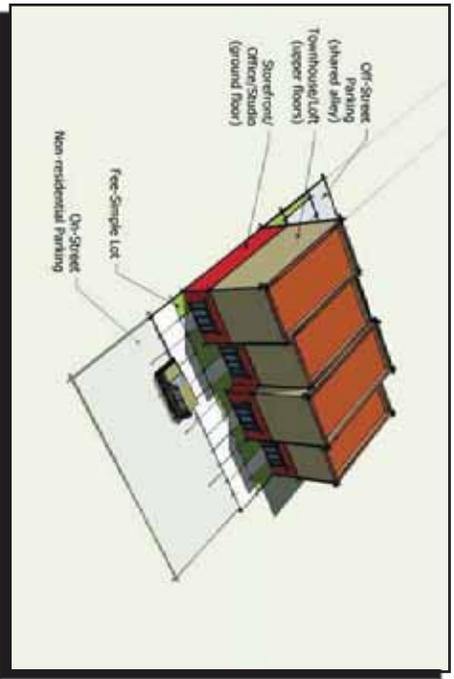
**Townhome Development Standards:**

- Townhomes shall be set back between five (5) to (15) feet from the back of the sidewalk. Setbacks for individual units may vary within a single block of townhomes to break up the facade and delineate individual units. This area between the sidewalk and the building facade shall contain only steps, front porches or stoops, balconies, or landscaping.

- Each individual townhome shall have either a front porch or a front stoop between the front facade and the sidewalk.

- The front entrance of each townhome unit shall be above the average grade of the sidewalk directly in front of it to a maximum of six (6) feet above grade.

**Townhome Lot Layout**



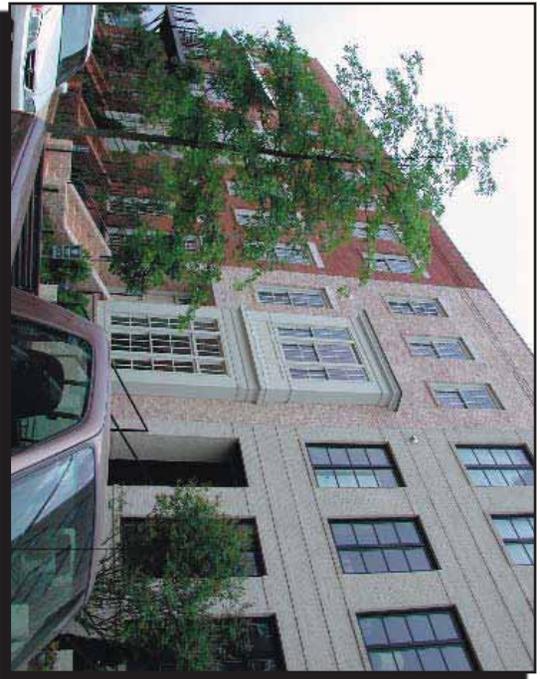
**Live/Work Lot Layout**

- All townhome parking shall be hidden behind or within individual units. Access to parking is permitted only via an alley or private drive located behind the units. Garages may not face the public street.

**Live/Work Development Standards:**

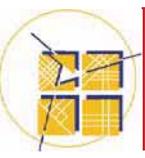
- All off-street parking shall be hidden behind or within individual units. Access to parking is permitted only via an alley or private drive located behind the units. Garages may not face the public street.

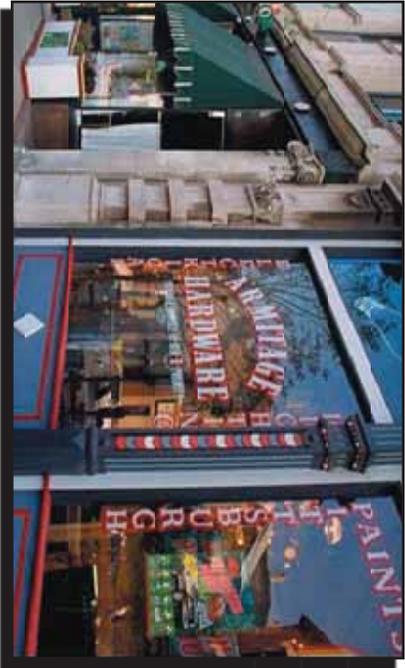
- The front entrance to each unit shall be at grade opening directly onto the public sidewalk or a public space adjacent to the public sidewalk.



**Architectural Features**

**16 TOWNHOMES & LIVE/WORK UNITS**





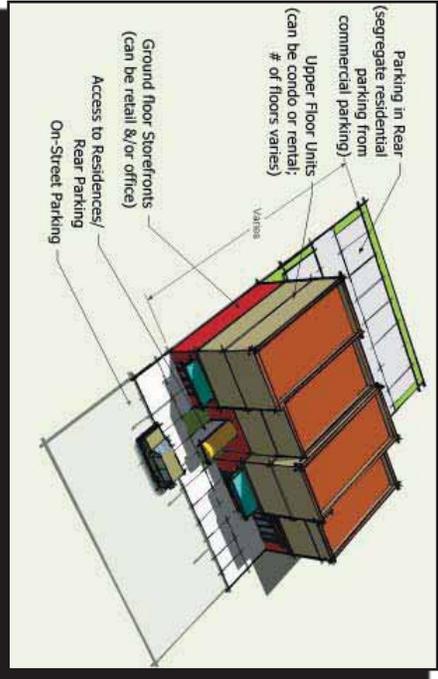
**65% of ground-floor facade shall be storefront.**

**Development Standards:**

- Ground-floor commercial and retail uses shall have entrances at grade opening directly onto the public sidewalk or a public space adjacent to the public sidewalk.
- Canopies over retail and commercial entrances and/or windows shall be mounted at a single consistent height for each building.
- The size of leased commercial space shall vary within each development to allow for a diversity of tenants.
- A minimum of 65% of the ground-floor facade of mixed-use and commercial buildings shall be clear and unlinted transparent storefront.
- Pedestrian access shall be provided from parking behind buildings to the public sidewalk through the ground floor of the building or via sidewalks between buildings.
- The primary entrance for all upper story uses shall be clearly visible from the street and shall face the street; if a building fronts more than one street, the primary entrance should face the street with the highest classification as follows:  
 Primary: Peachtree Road and Apple Valley Road;  
 Secondary: Dresden Drive, North Druid Hills Road, Hermonce Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Drive, Brookhaven Place; Tertiary: new streets and other existing streets.
- The combined area of any and all signage shall not exceed 10% of the main building façade.



**Architectural Features**



**Lot Layout**

**BROOKHAVEN - PEACHTREE DESIGN GUIDELINES**

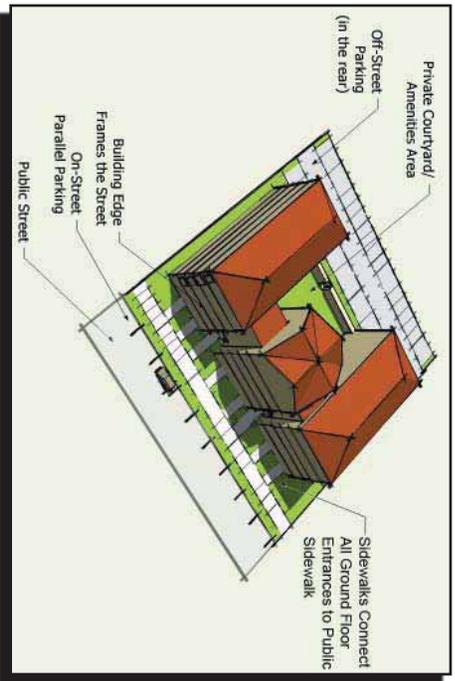
**17 COMMERCIAL & MIXED-USE**

Prepared By: Urban Collage, Inc.  
 Prepared For: Brookhaven - Peachtree LCI

**DRAFT**

March 2007

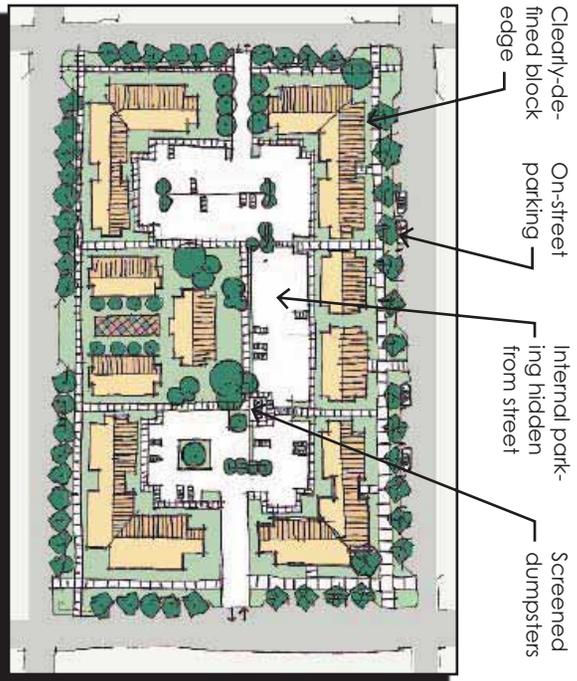




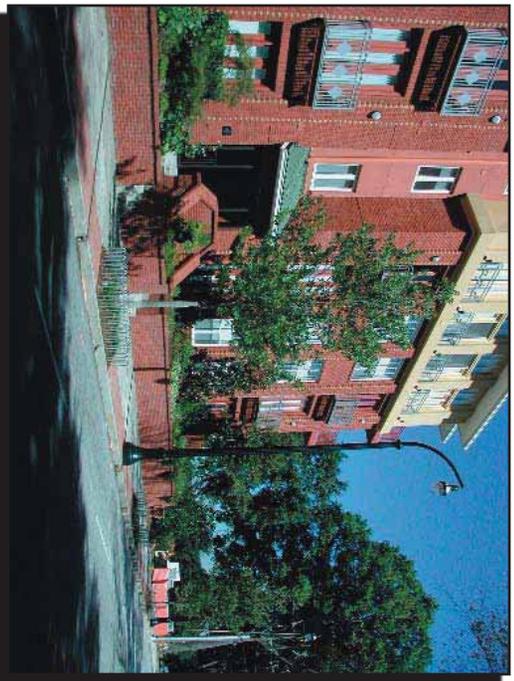
**Development Standards:**

- Ground-floor residential units that face the street shall have individual entrances with a stoop or porch between the sidewalk and the building facade. A sidewalk shall connect all ground floor entrances to the public sidewalk.
- Buildings shall be set back between five (5) to (15) feet from the back of the sidewalk. This area between the sidewalk and the building facade shall contain only steps, front porches or stoops, balconies, or landscaping.
- The primary entrance for upper story units shall be clearly visible and inviting from the street and shall face the street. If a building fronts more than one street, the primary entrance should face the street with the highest classification as follows:  
 Primary: Peachtree Road and Apple Valley Road;  
 Secondary: Dresden Drive, North Druid Hills Road, Herman Drive, Ashford-Dunwoody Road, Lanier Drive, Osborne Drive, Brookhaven Place, Tertiary: new streets and other existing streets.

**Lot Layout**

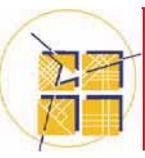


**Lot Features**



**Architectural Features**

**18 MULTI-FAMILY**





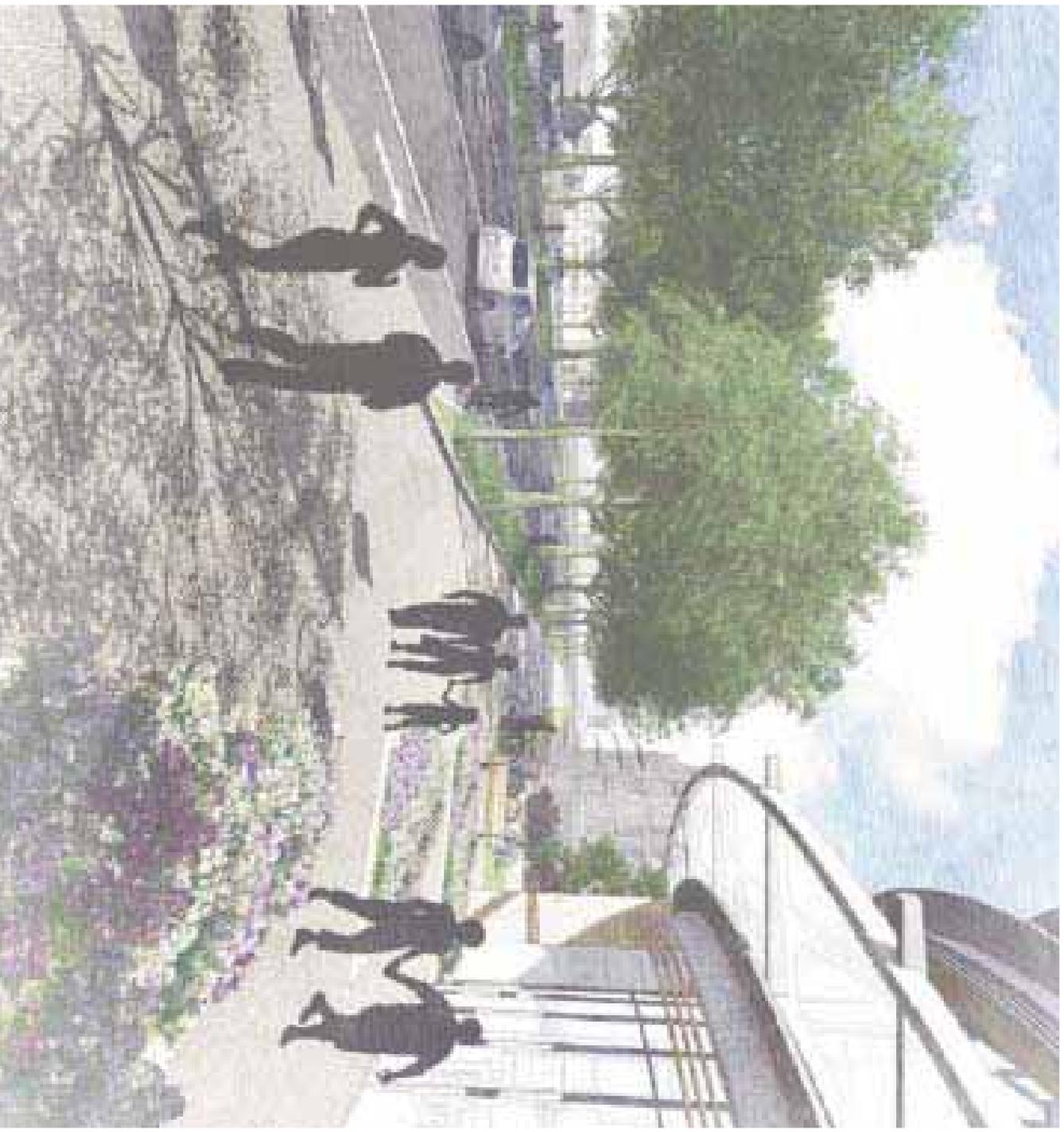
## Appendix B: Brookhaven-Oglethorpe MARTA Charrette - Summary of Neighborhood Engagement

# Brookhaven Oglethorpe

## MARTA Charrette

Summary of Neighborhood Engagement

October 2013



## Mixed Use/ Residential Mid Rise Development

The site will be anchored by a mix of uses that will include residential, commercial and professional opportunities.

The images that received the most positive feedback include similar elements, such as:

- Active ground floor retail areas that are visible from the public right-of-way (street or sidewalk) with at-grade entrances that open directly onto a public sidewalk
- Ground floor facades dominated by plate glass for the display of merchandise or activities inside the space
- Pedestrian zones adjacent to ground floor retail with appropriate landscaping and amenities
- Buildings pulled to the street front (0 lot line development) at the sidewalk



- Plaza spaces and courtyards which are accessible to ground floor retail and on interior spaces of a project
- Quality finish materials, such as brick, stucco or masonry
- Architectural interest and diversity in fenestration, cornice lines, corbelling and other variations in the facade such as balconies and bays
- Upper level residential, office or gathering uses
- Trees, greenspace, and public gathering areas
- Structured parking completed shielded from public view, either through underground parking or being constructed on the interior of a building



The overlay will allow development up to 125' on the MARTA site with certain incentives. Most images that scored well on the survey reflected buildings that were mid-rise scale development (4-6 stories). When given options for high rise (10-12 stories), community members did not respond favorably. Overall the respondents seemed to accept that the MARTA property was an appropriate location for higher density development, but exception was noted by some participants. Given its proximity to the civic center of Brookhaven care and consideration should be given to aesthetics and long term value of the development on the site, in addition to its carrying capacity for development.

## **Mixed Use/ Residential Low Rise Development**

The MARTA parking lot on the Peachtree Road side of the station offers limited developable area. Consideration was given to lower scale mixed used development styles for this site. Preference was still shown for similar features, including active ground floor areas and plazas that include greenspace amenities. Upper floors could contain residential uses, or office/ professional uses given its proximity to Peachtree Road. Residents were very clear that they did not want strip mall type development in that area.

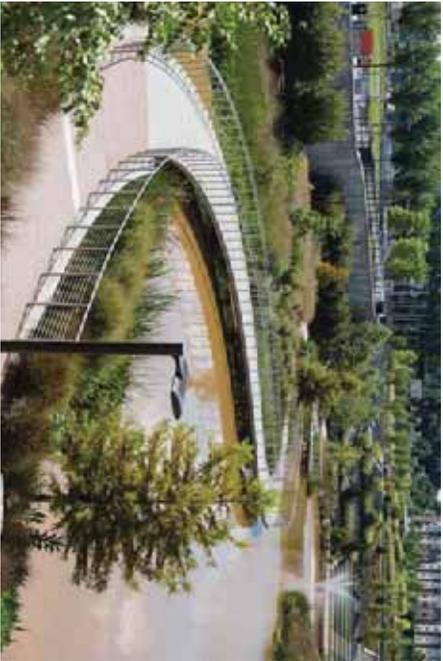


## **Office/ Institutional Development**

The site will offer the potential for multiple development pads. This could be under a master developer or through multiple developments. Though the overall site plan should strive for a cohesive architectural aesthetic, image preference results did indicate positive feedback for modern design elements including glass, steel and concrete structures for Office/ Institutional Developments.



## Stormwater/ Open Space Development



A strong desire for the inclusion of open space on the site was articulated by the community. The community members particularly stressed the need for a public gathering space, both through casual use areas and programmed features.

- Activate open spaces by creating opportunities for community, such as splash pads for children and gathering places for adults
- Avoid duplication of other open space amenities that are constructed or contemplated nearby (such as playgrounds)
- Develop stormwater retention areas into a community asset
- Use multiple types of green infrastructure to capture stormwater and minimize the impact of run-off from the site
- Combine modern and traditional features
- Combine hardscape and landscape elements
- Offer a variety of opportunities for informal gathering and passive recreation, but do not include programmed or managed spaces such as an amphitheater



# Mixed Use / Residential Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

### Residential Option 1:

Vertical Mixed Use- Retail/Residential



85%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

### Residential Option 2:

Vertical Mixed Use- Retail/Residential



21%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

### Residential Option 3:

Mixed Use- Retail/Residential



95%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

### Residential Option 4:

Mixed Use- Retail/Residential



64%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

### Residential Option 5:

No Retail



30%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

### Residential Option 6:

No Retail



33%

# Mixed Use / Residential Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Residential Option 7:

Mixed Use



43%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Residential Option 8:

Traditional



52%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Residential Option 9



82%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Residential Option 10

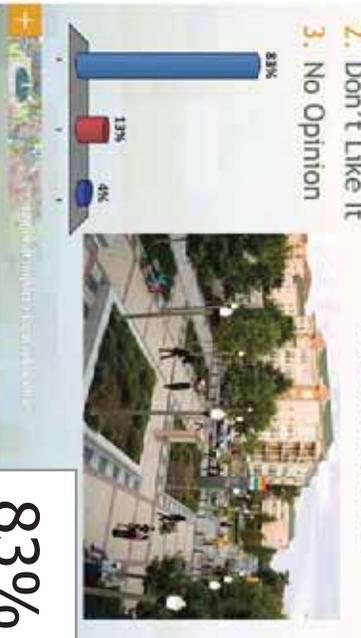


50%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Residential Option 11



83%

# Office / Institutional Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 1:



58%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 2



12%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 3



61%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 4



21%



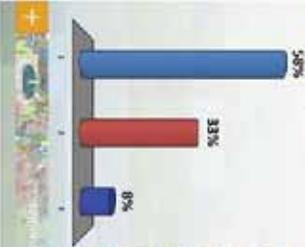
## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 5



58%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 6



33%



# Office / Institutional Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Office Building Option 7

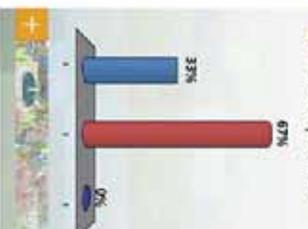


42%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Institutional Option 1

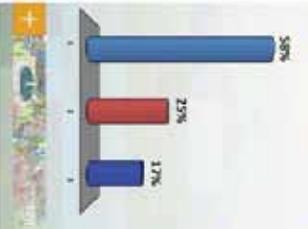


33%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Institutional Option 2



58%

# Lower Density Retail Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Lower Density Retail Option 1

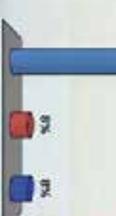


26%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Lower Density Retail Option 2



83%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Lower Density Retail Option 3



0%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Lower Density Retail Option 5

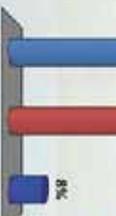


63%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Lower Density Retail Option 6

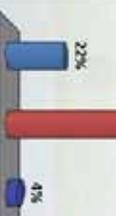


44%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Lower Density Retail Option 7



22%

# Stormwater Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Storm Water Retention Option 1



100%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Storm Water Retention Option 2



42%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Storm Water Retention Option 3



54%

# Open Space Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 1



83%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 4



57%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 3



54%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 5



33%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 6



44%

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 7



67%

# Open Space Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Open Space Option 8



67%

# Parking Voting Results

## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Car Parking Facility Option 1



58%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Car Parking Facility Option 3



92%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

Car Parking Facility Option 2



12%



## What Do You Think?

1. Like It
2. Don't Like It
3. No Opinion

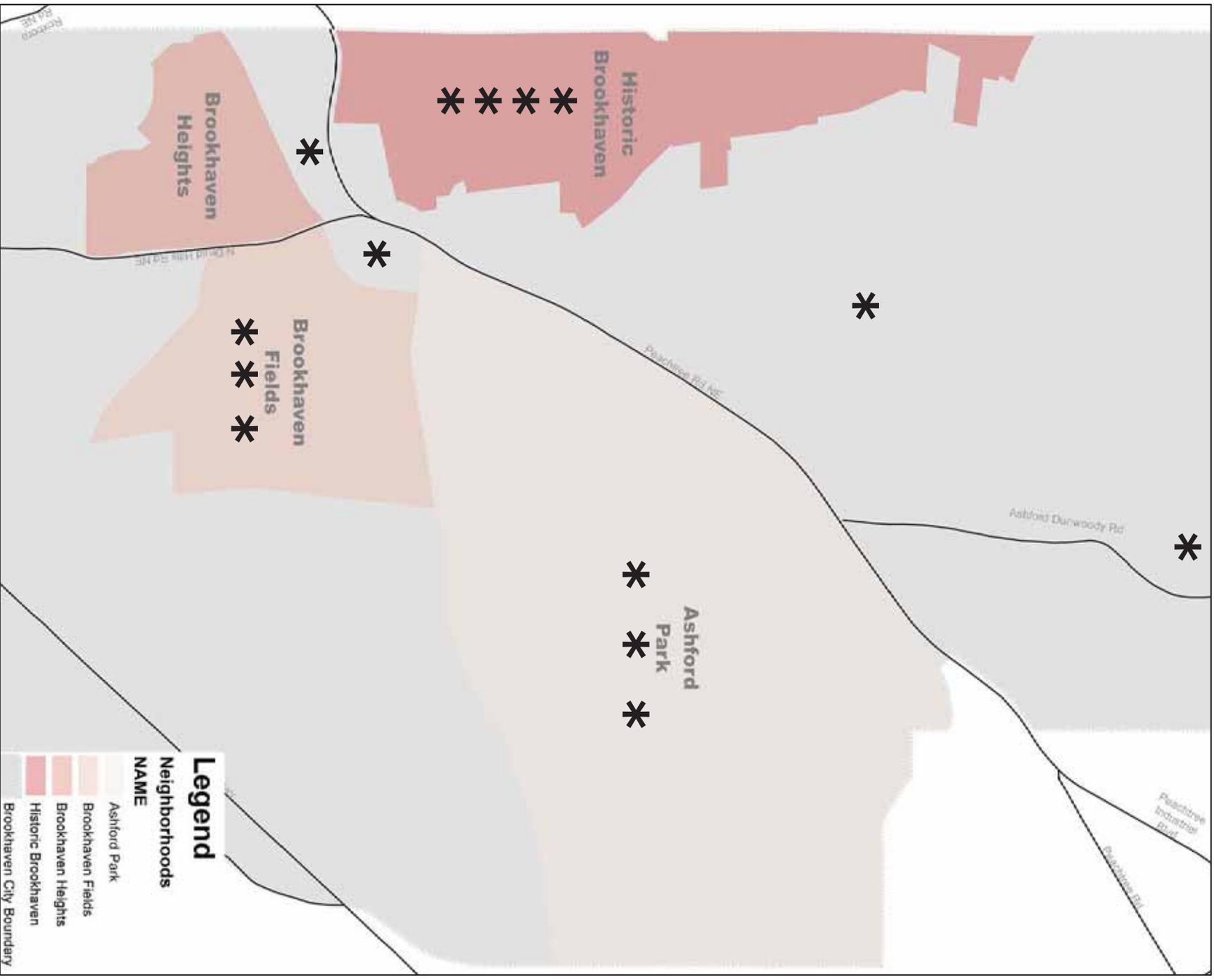
Car Parking Facility Option 4



17%



# Your Perspective...Neighborhood Representation



\* indicates representation of a participant in the neighborhood engagement sessions

# Share Your Thoughts...The Zoning Overlay?

## Height and Density

*Density – if not on Peachtree, where?*

*Does the 125 foot height regulation come from FAA?*

*Yes! To Density; Go even further by increasing the density bonus*

*Not that worried about height – go big and get it done*

*Height privacy issues- effects privacy issues of nearby houses*

## Architectural Standards

*Use Landscaping and Trees to maintain current character*

*Quality Design Guidelines as Zoning Conditions*

## Mix of Uses

*New Grocery – easier to get to than Kroger*

*Want something that residents can come to as a destination*

*Noise concerns – restaurants/ bars- greater buffer from restaurants*

*No restaurants/ no live music/ plenty of apartments along Dresden no need for more*

## Other

*Shared Parking for small lots – Elijay and Coldwell and Dresden; city should be responsible for building shared parking facility*

# Share Your Thoughts...Neighborhood Concerns?

## Traffic and Congestion

Majority of cut thru traffic comes from Peachtree Dunwoody – having the removal of 400 toll may relieve cut thru

Can live with cut thru traffic for 1-2 hours each afternoon (left turn on Peachtree to E. Brookhaven)

Taking away left turn on Peachtree Road compounds problems

Clean up intersection markings and small widenings

Desire for walkability/ cycling

Apple Valley used by many but is a small road

Would like a pedestrian cut through to Town Brookhaven

Solve problems on side streets to fix problem on main streets

Not being able to turn left on E. Brookhaven

Increase connectivity in new neighborhoods; required developers to increase the grid

Provide pedestrian access across Fernwood Park

Promise sidewalks on Coldwell and Apple Valley; Coldwell is most important

Pedestrian overpass bridge over Peachtree Road at key intersections

Dysfunctional triangle/ can't see joggers at Brookhaven and E. Brookhaven

Need more sidewalks in Brookhaven Fields; increase connectivity

Building of Post Brookhaven and other apartment caused the problems of cut through traffic

Colonial cut through is a problem; the intersection has a light; not sure how to restrict cut through on public roads

Traffic calming through the neighborhoods in Historic Brookhaven

Improve sidewalks in the neighborhood to the MARTA station

Inadequate parking on Dresden/ blocks traffic

Not a fan of no left turn on Peachtree Road; section in front of MARTA is the worst

Brookhaven Drive is being used for thru traffic

Fix the triangle at Brookhaven/ E. Brookhaven; avoid the traffic at the time it is bad; the proposed cure (removing the left turn off Peachtree) is worse than the disease

Increase connectivity and walkability and multi-modal options

Just don't see other forms of transportation working

## Housing Affordability

Workforce Housing is okay; no subsidized housing; no HUD Section 8 housing

Need affordable housing near transit for aging people

Like MARTA's 20% rule; need mix of housing types integrated with market rate housing

## Buffers & Greenspace

Stay away from concrete towers; make sure you have greenspace; not like Lindberg Station

Fernwood Park – incorporate into vision as asset

Keep natural state of stormwater run-off to provide habitat

Independent study on conservation easement on blueline creek in Historic Brookhaven

Blueline creek in historic brookhaven needs to be protected; flooding concerns

New development needs serious stormwater attention

Pedestrian access at Fernwood Park; improve parkside park, particularly along Dresden

Insure that stormwater drainage is handled on site

Buffer Preservation 150'?

## Other

Too many of the stakeholders were elected officials; need more outreach to neighborhoods

Joggers are a big issue; sidewalks are broken

Any homes that get permits must consider sidewalks, ROW acquisition and widening

Long time before storage redevelops

Lots of people walk on Apple Valley; only new home permits should consider new home permits

Dunkin Donuts truck parking on sidewalk

Truck traffic going to Capitol City Club

People take Ashford-Dunwoody/ Clarimont Road to avoid 400 tolls; make Dresden/ N. Druid Hills a more local street

Not able to convert existing businesses

# Share Your Thoughts...MARTA Site Design?

## Site Design

*Lindberg is the negative image of MARTA TOD*

*Need trees incorporated into the development*

*Increase connectivity and the traditional grid network on site*

*Height concerns; 4 stories more appropriate; 8 stories affect neighborhood character*

*Vertical mixed use; ground floor retail upper residential*

*Light pollution associated with taller buildings (5 story +) affects wildlife*

*Uniform and consistency in MARTA property*

*Make the front MARTA lot ROW for Peachtree Road*

## Parking & Accessibility

*Connectivity to new development like parking toward tracks and not abutting roads*

*Strong access to Peachtree road from rear development area*

*Pedestrian connectivity and integration to neighborhoods*

*Drop off land in front with handicap parking and everything else in back*

## Community Safety

*Well lit and sense of activity*

*Comfortable for families*

*Used to be able to see drug deals off Windsor Drive; keep that culture out of here*

*Safety for children when Brookhaven/E. Brookhaven is used as a cut through*

*Safety for joggers*

*Request for sidewalks; not enforcing regulations for developers to install sidewalks*

*Need sidewalks on Coldwell and Apple Valley*

*Making it safer for bike/ ped will help traffic*

*Child hit by car on Coldwell; Oak Lawn and Sunland has no left turn, but not enforced*

## Other

*Generally positive about the idea of a city center*

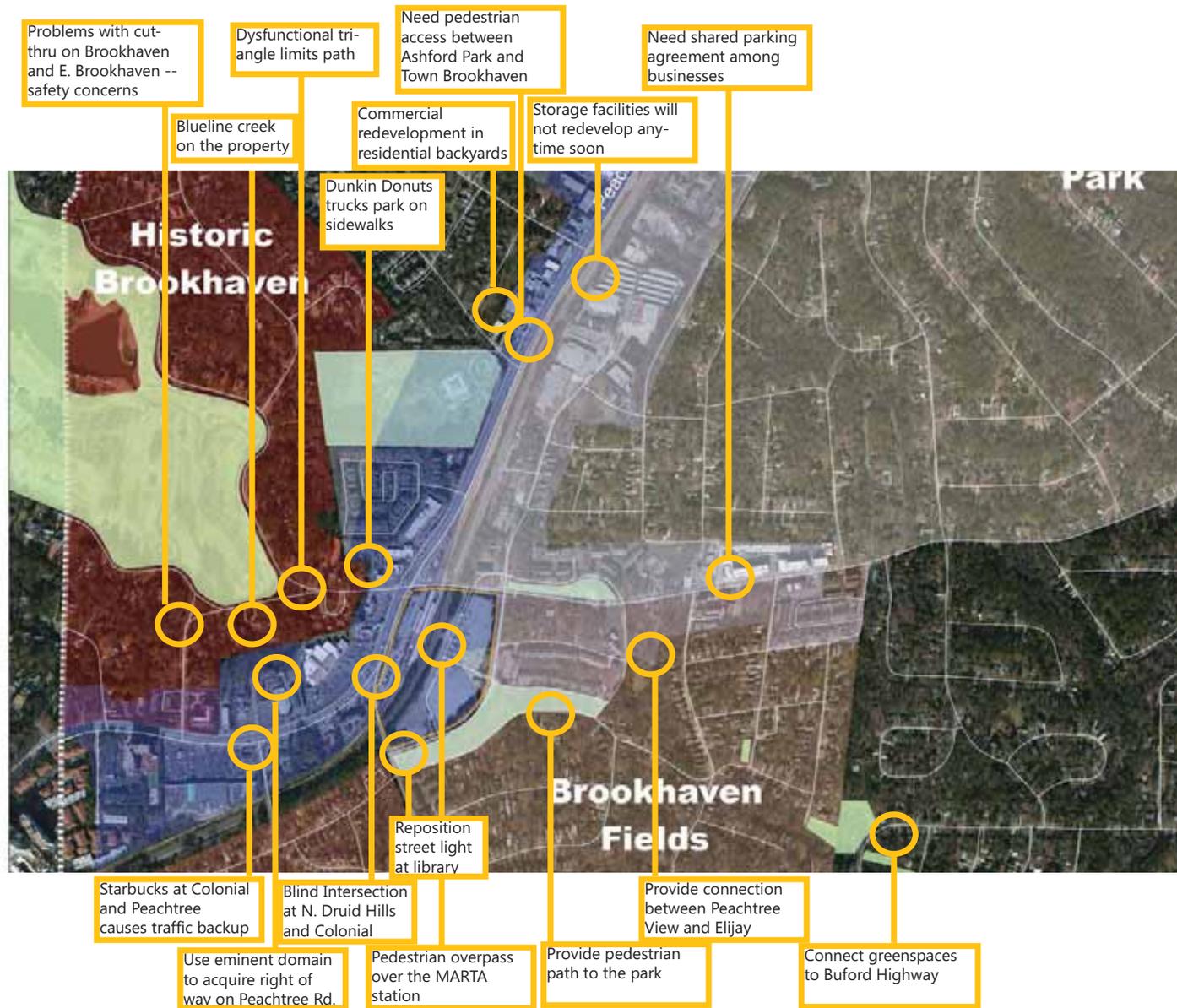
*Agree with the idea of a city center; gathering space; outdoor venue space*

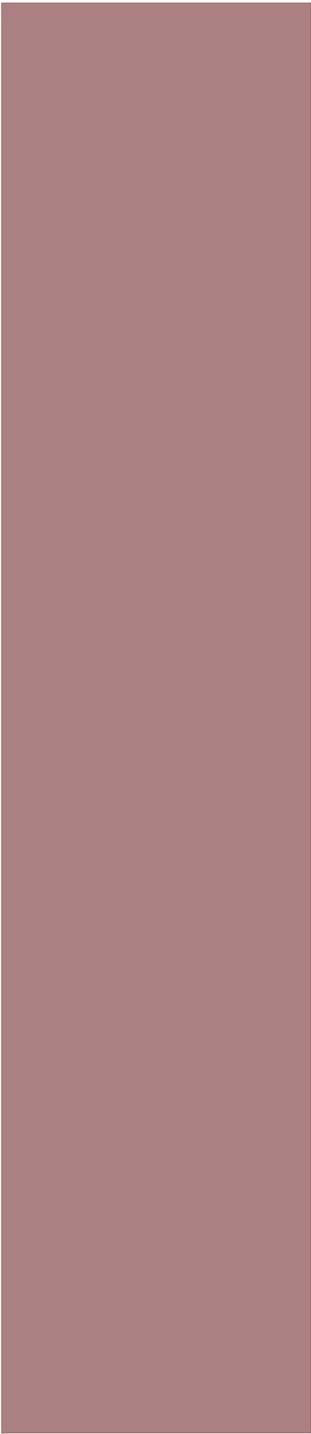
*Lively uses on site*

*Master developer for entire site controlling image and destiny*

*Realign Elijay and Coldwell; land swap with property owners*

# BROOKHAVEN/OGLETHORPE CHARRETTE





**Appendix C: Stormwater Facility and City Park Full Concept Report, Calculations, and Cost Summary**

# Stormwater Facility and City Park Recommended Design Options

*Brookhaven-Oglethorpe MARTA Charrette – 10/21/2013*

---

## Option 1:

### *Design Features/Elements:*

- Dam remains in existing location
- Modify or replace outlet control structure to create wet detention pond:
  - 0.3 acres open water
  - 1.3 acres BMP footprint
- Enhance approximately 200 linear feet of stream and riparian buffer in pond
- Enhance approximately 650 linear feet of stream and riparian buffer in Fernwood Park
- Creation of approximately 3,000 linear feet of low-impact pedestrian trail including benches, lighting, and pet clean-up stations
- Perform vegetative management to remove invasive/undesirable species and landscaping through pond and Fernwood Park
- Install pre-treatment oil/grit separators at each catch basin

### *Summary of Benefits:*

- Provides 100% treatment of Water Quality and Channel Protection Volume in stormwater pond
- Stormwater pond provides some reduction in peak flows to receiving streams
- Stormwater pond provides approximately 0.3 acres of open water as a community amenity
- Combining Fernwood Park and stormwater pond into single, connected linear park for community green space
- Provides connectivity to surrounding environmental corridors
- Park is easily accessible from TOD facility as well from surrounding roads Apple Valley, North Druid Hills, Sylvan Circle and Fernwood Circle

### *Estimated Cost:*

\$991,000

## Option 2:

### *Design Features/Elements:*

- Relocate dam approximately 180 feet east to increase open water surface area
- Replace outlet control structure to create wet detention pond (0.9 acre open water/1.8 acres BMP footprint)
- Enhance approximately 200 linear feet of stream and buffer in pond
- Enhance approximately 470 linear feet of stream and buffer in Fernwood Park
- Creation of approximately 3,000 linear feet of low-impact pedestrian trail including benches, lighting, and pet clean-up stations
- Perform vegetative management to remove invasive/undesirable species and landscaping through pond and Fernwood Park
- Install pre-treatment oil/grit separators to at each catch basin

### *Summary of Benefits:*

- Provides 100% treatment of Water Quality and Channel Protection Volume
- Maximizes reduction of peak flows to downstream receiving streams
- Increased open water for pond amenity
- Combining Fernwood Park and stormwater pond into single, connected linear park for community green space
- Provides environmental corridor connectivity
- Park is easily accessible from TOD facility as well from surrounding roads Apple Valley, North Druid Hills, Sylvan Circle and Fernwood Circle

### *Estimated Cost:*

\$1,406,000

### Option 3:

#### *Design Features/Elements:*

- The 95th percentile storm event is captured on the site and retained, infiltrated, evapotranspired, or reused to the maximum extent practicable based on soil type and on building and landscaping needs.
- Maintain a composite curve number of 65 or lower for all runoff exiting the development
- Low-Impact Design and Green Infrastructure elements to be considered include:
  - Infiltration trenches/vegetated swales
  - Green roof
  - Bioretention Areas/ Rain Gardens
  - Stormwater Cisterns
  - Urban Forestry
  - Permeable Pavers
  - Underground storage and reuse
- Modify or replace outlet control structure to create wet detention pond
  - 0.3 acres open water
  - 1.3 acres BMP footprint
- Enhance approximately 200 linear feet of stream and buffer in pond
- Enhance approximately 650 linear feet of stream and buffer in Fernwood Park
- Creation of approximately 3,000 linear feet of low-impact pedestrian trail including benches, lighting, and pet clean-up stations
- Perform vegetative management to remove invasive/undesirable species and landscaping through pond and Fernwood Park
- Install pre-treatment oil/grit separators to at each catch basin

#### *Summary of Benefits:*

- Incorporation of LID/GI features can be used to create aesthetically pleasing green amenities within TOD facility
- Provides 100% treatment of Water Quality and Channel Protection Volume
- Maximizes reduction of peak flows to downstream receiving streams
- Combining Fernwood Park and stormwater pond into single, connected linear park for community green space
- Provides environmental corridor connectivity
- Park is easily accessible from TOD facility as well from surrounding roads Apple Valley, North Druid Hills, Sylvan Circle and Fernwood Circle

#### *Estimated Cost:*

\$923,000\*

\*Note that cost of LID/GI elements are not included in the estimated cost. The LID/GI elements will be applied during development of upland areas. The estimated cost for option 3 is lower than option 1 because the size of the pond required is smaller due to LID/GI implementation in the watershed.

**WQV Calculations:**

$$WQV = 1.2 * R_v * A$$

$$R_v = 0.05 + (0.09 * I)$$

I (%)	80.00
R <sub>v</sub>	0.77
A (ac.)	27.30
WQV (ac-ft)	2.10
WQV (cu.ft.)	91,567

**CPV Calculations:**

$$Q = (P - 0.2S)^2 / (P + 0.8S)$$

$$S = (1000/CN) - 10$$

P (in.)	3.36	1-yr 24-hr
CN	80	
S	2.50	
Q (in.)	1.53	
A (ac.)	27.30	
CPV (cu.ft.)	151,230	

**100-yr Volume Calculations:**

$$Q = (P - 0.2S)^2 / (P + 0.8S)$$

$$S = (1000/CN) - 10$$

P (in.)	10.4	100-yr 24-hr
CN	80	
S	2.50	
Q (in.)	7.90	
A (ac.)	27.30	
CPV (cu.ft.)	783,282	

Require user input  
Calculated values

OPTION I

Pond Restoration	Cost
Base Construction Cost	\$ 389,000
Erosion Control (5%)	\$ 19,450
Construction cost	\$ 408,450
Contingency (20%)	\$ 81,690
Total Construction Cost	\$ 490,140
Design	\$ 85,000
Construction Services	\$ 45,000
<b>ESTIMATED TOTAL COST</b>	<b>\$ 620,140</b>

Estimated Cost

\$ 991,000

Stream Restoration	Cost
Base Construction Cost	\$ 210,000
Erosion Control (5%)	\$ 10,500
Construction cost	\$ 220,500
Contingency (20%)	\$ 44,100
Total Construction Cost	\$ 264,600
Design	\$ 66,300
Construction Services	\$ 39,780
<b>ESTIMATED TOTAL COST</b>	<b>\$ 370,680</b>

OPTION II

Pond Restoration	Cost
Base Construction Cost	\$ 757,000
Erosion Control (5%)	\$ 37,850
Construction cost	\$ 794,850
Contingency (20%)	\$ 158,970
Total Construction Cost	\$ 953,820
Design	\$ 100,000
Construction Services	\$ 45,000
<b>ESTIMATED TOTAL COST</b>	<b>\$ 1,098,820</b>

Estimated Cost

\$ 1,406,000

Stream Restoration	Cost
Base Construction Cost	\$ 174,000
Erosion Control (5%)	\$ 8,700
Construction cost	\$ 182,700
Contingency (20%)	\$ 36,540
Total Construction Cost	\$ 219,240
Design	\$ 50,000
Construction Services	\$ 37,500
<b>ESTIMATED TOTAL COST</b>	<b>\$ 306,740</b>

OPTION III

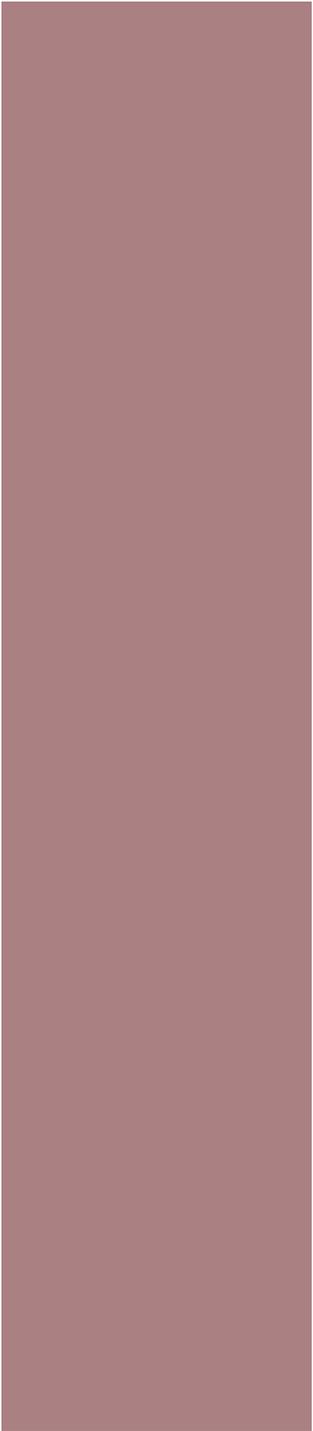
Pond Restoration	Cost
Base Construction Cost	\$ 335,000
Erosion Control (5%)	\$ 16,750
Construction cost	\$ 351,750
Contingency (20%)	\$ 70,350
Total Construction Cost	\$ 422,100
Design	\$ 85,000
Construction Services	\$ 45,000
<b>ESTIMATED TOTAL COST</b>	<b>\$ 552,100</b>

Stream Restoration	Cost
Base Construction Cost	\$ 210,000
Erosion Control (5%)	\$ 10,500
Construction cost	\$ 220,500
Contingency (20%)	\$ 44,100
Total Construction Cost	\$ 264,600
Design	\$ 66,300
Construction Services	\$ 39,780
<b>ESTIMATED TOTAL COST</b>	<b>\$ 370,680</b>

Estimated Cost  
\$

923,000

## Appendix D: Renderings - Streetscape and Regulating Plans













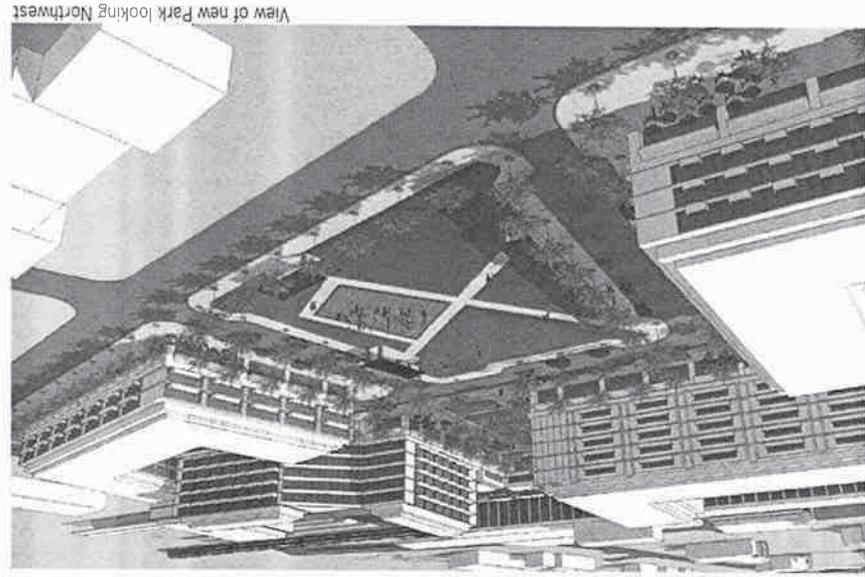
REGULATING PLAN A



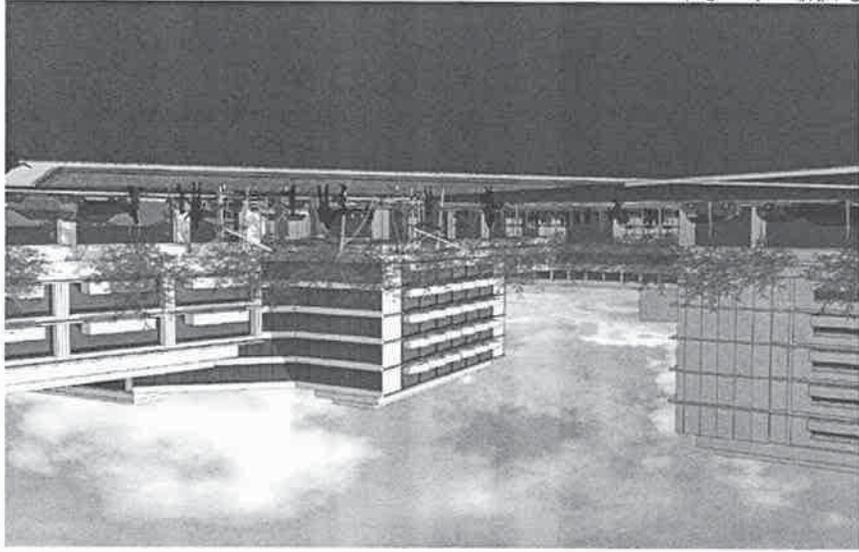


REGULATING PLAN B

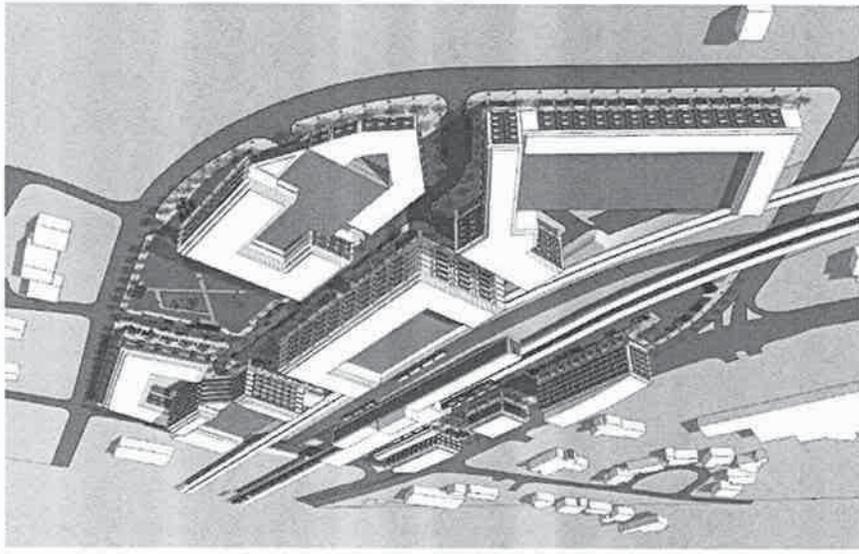




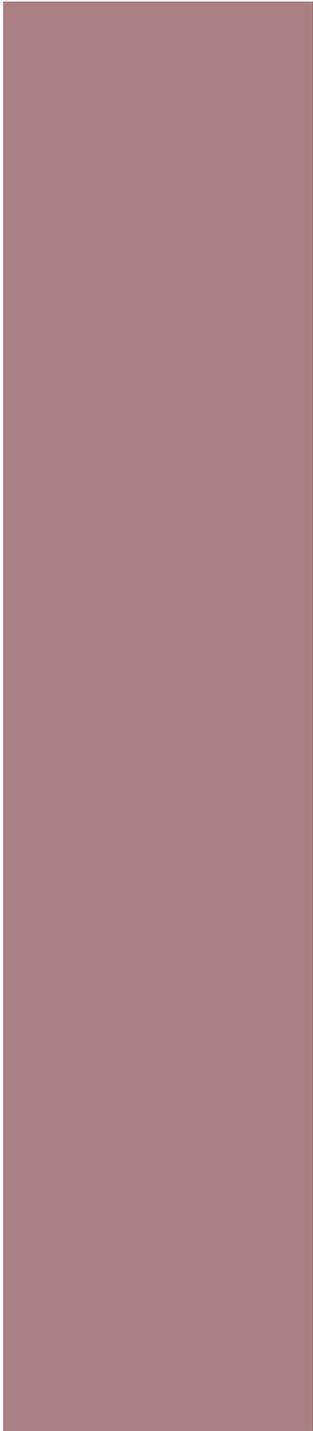
View of new Park looking Northwest



Detail View of new Park



## Appendix E: Background Session Presentations



# BROOKHAVEN-OGLETHORPE MARTA STATION CHARRETTE

BACKGROUND SESSION 1: UNDERSTANDING THE BROOKHAVEN-PEACHTREE  
OVERLAY AND LCI STUDY  
OCTOBER 7, 2013



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# Background Session Agenda

- I. Welcome & Introductions
- II. Schedule
- III. Charrette Overview
- IV. LCI Process
- V. Overlay Possibilities
- VI. MARTA TOD Guidelines
- VII. Walkable Urbanism-WalkUPs
- VIII. Questions & Answers



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# Project Team

## Southface

- Robert Reed, Alex Trachtenberg, Gray Kelly, Clay Elliot, Maggie Baynham, Dustin Norton and David Knorr

## City of Brookhaven

- Mayor J. Max Davis, Rebecca Chase Williams, Bates Mattison, Jim Eyre, Joe Gebbia, Marie Garrett, Susan Canon

## Atlanta Regional Commission

- Dan Reuter, Jared Lombard and Allison Duncan

## Atlanta/National Board of Realtors

- Lennie Shewmaker

## MARTA

- John Crocker and Jason Ward

## Design Team

- Eric Bosman, Bob Munger, Eric Parker, Clarence Vinson, Bryan Long, Michael Roberts, Christina Davis, Dave Skelton, Dwayne Tedder, Glenn Kurtz, Jason Frost, Bill Bassett, Joe Palladi, Mike Lobdell, Joel Putterman, Paul Moore, John Ladson, Jay Scott, John Funny, Laurel Hart, Joe Collums, Jack Honderd, Mike Elliot, Art Lomenick

# Stakeholders

- Alison Weissinger
- Andrea Botham (andrea.botham@yahoo.com)
- Arthur Freeman
- Bates Mattison (bates.mattison@brookhavenga.gov)
- Bennett Sands (brs@woodpartners.com)
- Bill Hightower
- Carmen Hutchins
- Charlie Heard
- Dan Woodley (dwoodley@woodleyaffiliates.com)
- Dick Williams
- Elaine Boyer
- Erik Steavens
- Frank Clementi
- G. Douglas Dillard
- Gary Yandura
- Gregory A. Miller
- Hope Bawcom (hopebawcom@gmail.com)
- J. Max Davis
- Jack Honderd (jkhonderd@gmail.com)
- Jeff Landau
- Jeff Rader
- Jennifer Harper
- Jerry Cooper
- Jim Bacchetta
- Jim Eyre
- Joe Collums (joe.collums@dca.ga.gov)
- Joe Gebbia
- John Funny
- John Lundeen III
- John Rhett (jtr1117@gmail.com)
- John Speros
- Julie Self (julie@atlantalocalfood.org)
- Kathy Forbes
- Kim Gokce (Kimgokce@gmail.com)
- Laurel Hart (laurel.hart@dca.ga.gov)
- Lawrence Schall (lschall@Oglethorpe.edu)
- Leilani Jones
- Lennie Shewmaker, J.D. (lennie@atlcb.com)
- Lisa Martinez
- Malory Atkinson (maloryhatkinson@gmail.com)
- Maria Duarte
- Marie Garrett
- Mark J. Miller
- Mark Kick

- Michael Horan
- Michel Arnette (michel@havenrestaurant.com)
- Mike Elliot (melliot@mindspring.com)
- Mike Jacobs
- Norton, Ruthie T.
- Patti Pearlberg
- Rebecca Chase Williams
- Roarke Donnelly
- Robert Adamo
- Robin Ikeda
- Ronnie Mayer (debbie@debbieleonard.com)
- Russell J Mitchell (russelljmittchell@gmail.com)
- Sandy Althomsons
- Scott Mathis
- Shane Day Boyer
- Stan Segal
- Susan Canon
- Susan Coker (susan.coker@comcast.net)
- Thomas Sinks
- Tim Nama
- Tom Dalia
- Vikram Mehra (Vikram.Mehra@hines.com)
- Walt Ehmer (waltehmer@comcast.net)
- Wendy S. Butler (wendy@wsbutlerlaw.com)
- Will Shearer (wshearer@uniteddistinc.com)



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# Brookhaven-Oglethorpe MARTA Station Transit Oriented Development Charrette

All sessions held at Oglethorpe Presbyterian Church, 3016 Lanier Drive Atlanta, GA 30319



## Background Sessions

**Monday, October 7**  
6:30-8:30 PM

- ◆ "Those who cannot remember the past are condemned to repeat it": Understanding the Brookhaven-Peachtree Overlay and LCI Study

**Monday, October 14**  
6:30-8:30 PM

- ◆ Transportation Planning: Putting the Pieces Together for a Multi-Modal Framework

**Thursday, October 17**  
6:30-8:30 PM

**Mellow Mushroom-Brookhaven**  
4058 Peachtree Rd NE Atlanta, GA 30319

- ◆ Workforce Housing: Good for the Community and Those Who Serve It

## Charrette Event

**Sunday, October 20 - Thursday, October 24**

**Sunday Evening**  
6:30-8:30 PM

- ◆ Community Meeting-Charrette Kickoff

**Monday - Thursday**  
8:00 AM - 5:00 PM

- ◆ Stakeholders meeting times TBD

**Thursday Evening**  
6:30-8:30 PM

- ◆ Community Meeting  
Presentation of Deliverables



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# Charrette Overview



## Edgewood-Candler Park MARTA Station Charrette

- Adaptive, reactive, efficient and engaging on multiple levels
- Open process including all interested parties to produce a feasible plan w/ minimal additional work

### Timeframe:

- Research, education and Charrette preparation = 1-9 mo.
- Charrette - 4 days
  - Public meeting held evening prior to 1<sup>st</sup> day of Charrette
- Deliverables = 2-4 mo. post Charrette
  - Report
  - Additional deliverables



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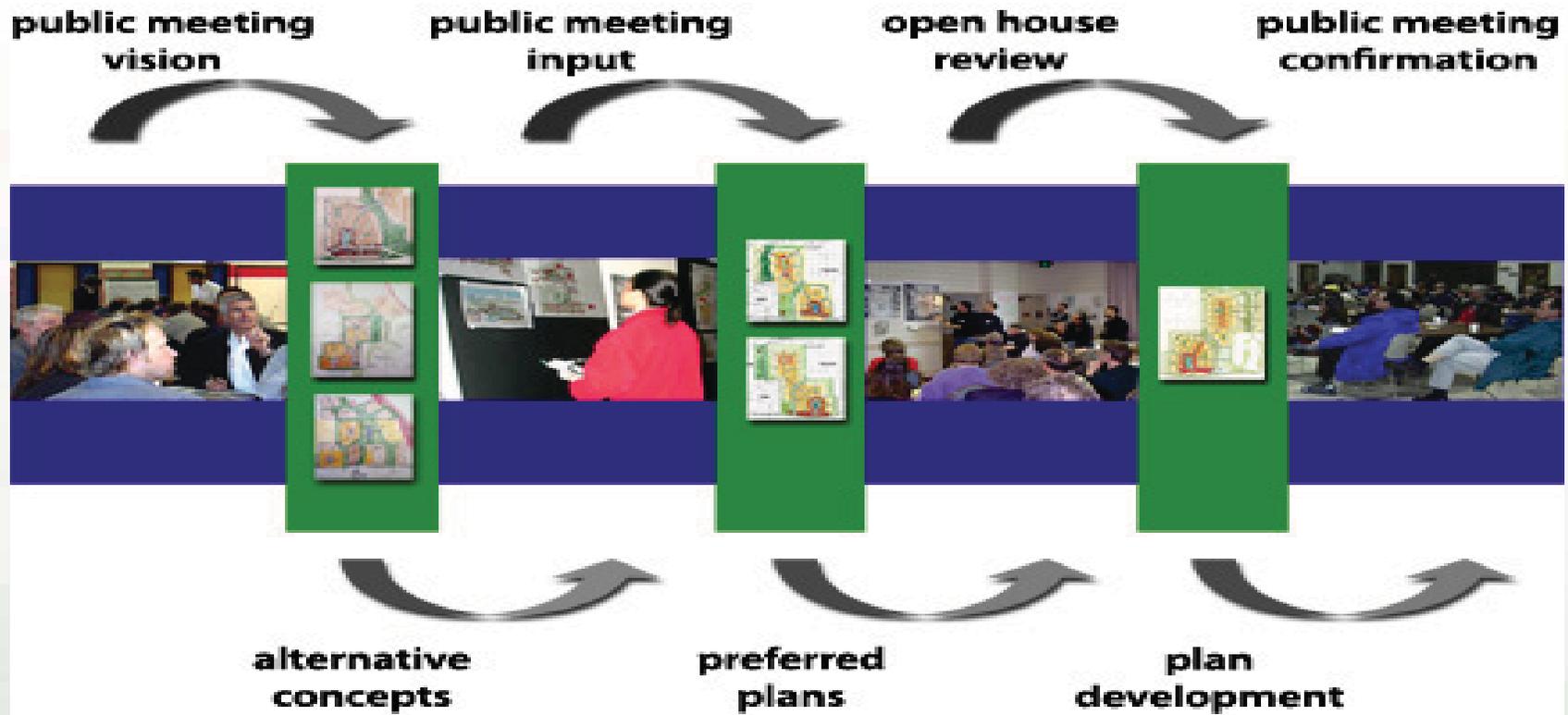


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# Charrette Event



- Project Team develops plan
- Stakeholders review
  - Stakeholders attend feedback meetings at critical decision making points
    - Stakeholders are not in attendance all the time
- Directly affected groups (citizens, business owners, etc.) are involved in short feedback loops or meetings

# Charrette Process

## Project Team

- Deliverables Description
- Guiding Principles
- Stakeholder Outreach
- Base Data Research & Analysis
- Pre-Charrette Brief and Charrette
- Values, Vision and Needs
- Alternatives
- Deliverables

June 2013

## Stakeholders

- Kickoff Meeting
- Stakeholder Interviews
- Educational Events (3)
- Charrette
- Meetings (3)
- Open Public Meeting
- Alternatives Public Meeting
- Final Public Meeting
- Communication and Follow-Up Meeting

October 2013

January 2014

# Livable Centers Initiative

- Created in 1999
- Mix of Uses: housing, retail, office, institutional, parks, schools, etc.
- Multi-modal focus with broad stakeholder involvement
- Contains 34/38 MARTA Stations
  - \$1.8 million Study Funds
  - \$24.2 million in transportation project funds
  - 3 TOD Studies in 2011/12



# Brookhaven LCI Study

## Community Planning Process-2006

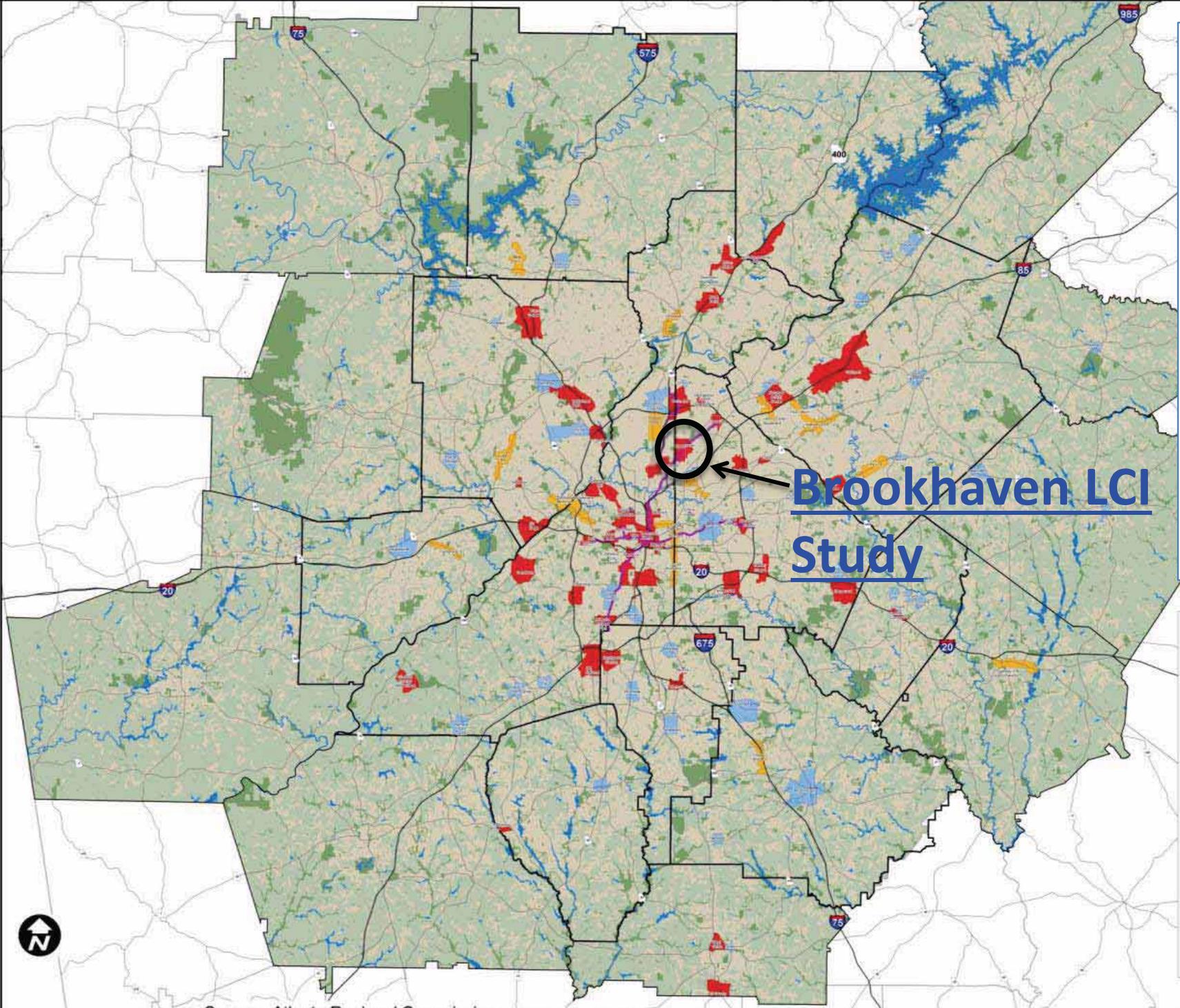
### Purpose and Intent:

- Encourage a mixed-use urban village
- Create a walkable, human-scale environment
- Increase connectivity
- Illustrate a vision for the MARTA Station and Brookhaven Corridors (Peachtree, Dresden, and North Druid Hills)
- Provide a variety of housing types
- Enhance community consistency (sidewalks, setbacks, design character etc.)



# 2000-2012 LCI Study Locations

11 counties (unincorporated) &  
53 cities with an LCI study



Atlanta	18
Cherokee	4
Clayton	6
Cobb	12
DeKalb	19
Douglas	2
Fayette	2
Fulton	18
Gwinnett	15
Henry	4
Rockdale	2
Outside	9
<b>Total</b>	<b>111</b>

**Brookhaven LCI Study**

**LCI Study Areas Types**

- Activity Center
- Corridor
- Town Center
- State Highways
- MARTA Rail
- Lakes & Ponds
- Rivers & Streams
- Developed
- Undevelopable
- Undeveloped

Source: Atlanta Regional Commission

# Brookhaven-Peachtree Overlay

LCI implementer

Purpose and intent:

- Mixed-use urban village
- Housing varieties
- Accessibility
  - MARTA, etc.
- Walkable, human-scale environment
- Efficient/effective use of land
  - MARTA, Peachtree
- Connectivity
  - Multi-modal
- Building compatibility
  - Setbacks, street frontage, façade, sidewalks, etc.



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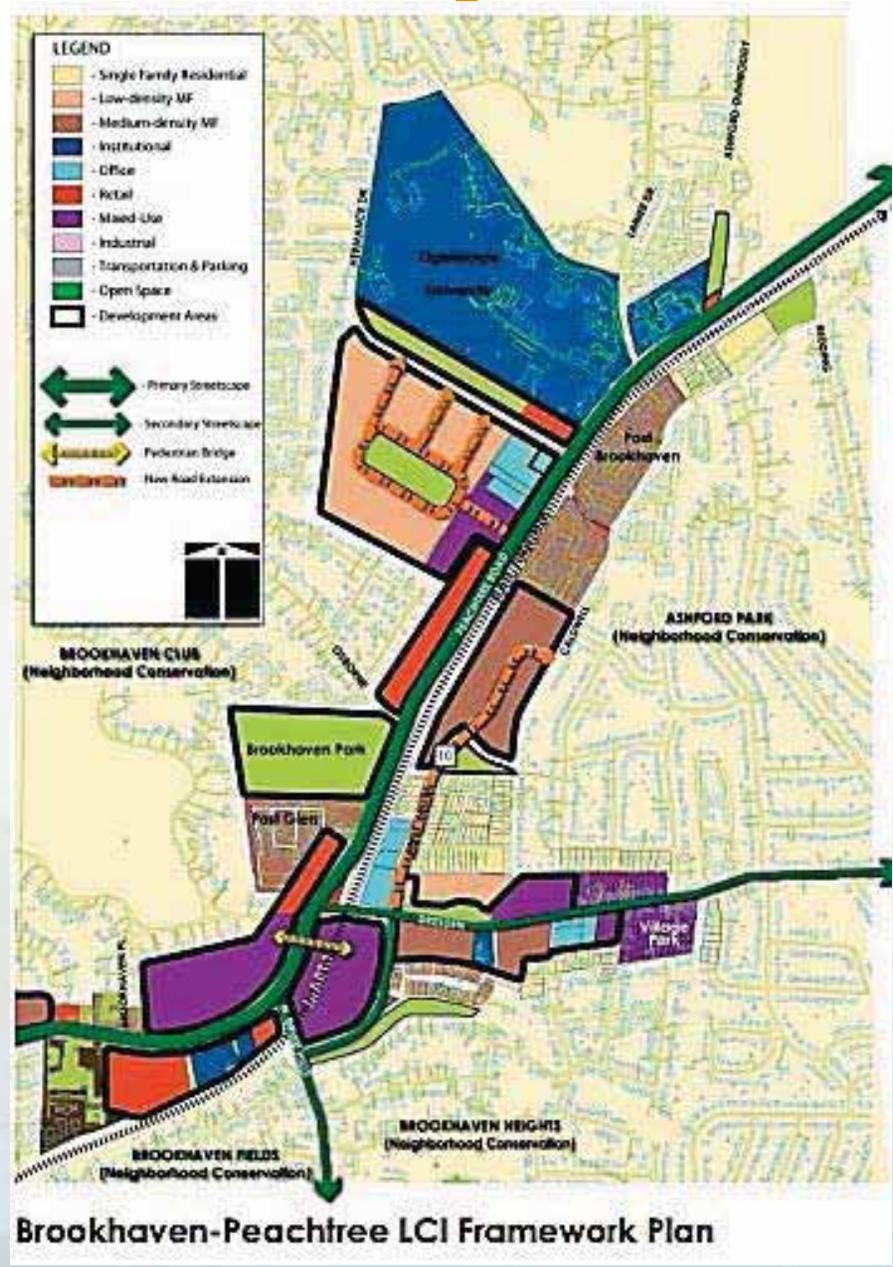
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# Brookhaven-Peachtree Overlay

## Site Design Standards

- Lot Area and Yard Requirements
- Transitional Buffer Zone and Height Requirements
- Design Requirements
- Streets and Sidewalks
- Parking



# Brookhaven-Peachtree Overlay

## Development Standards

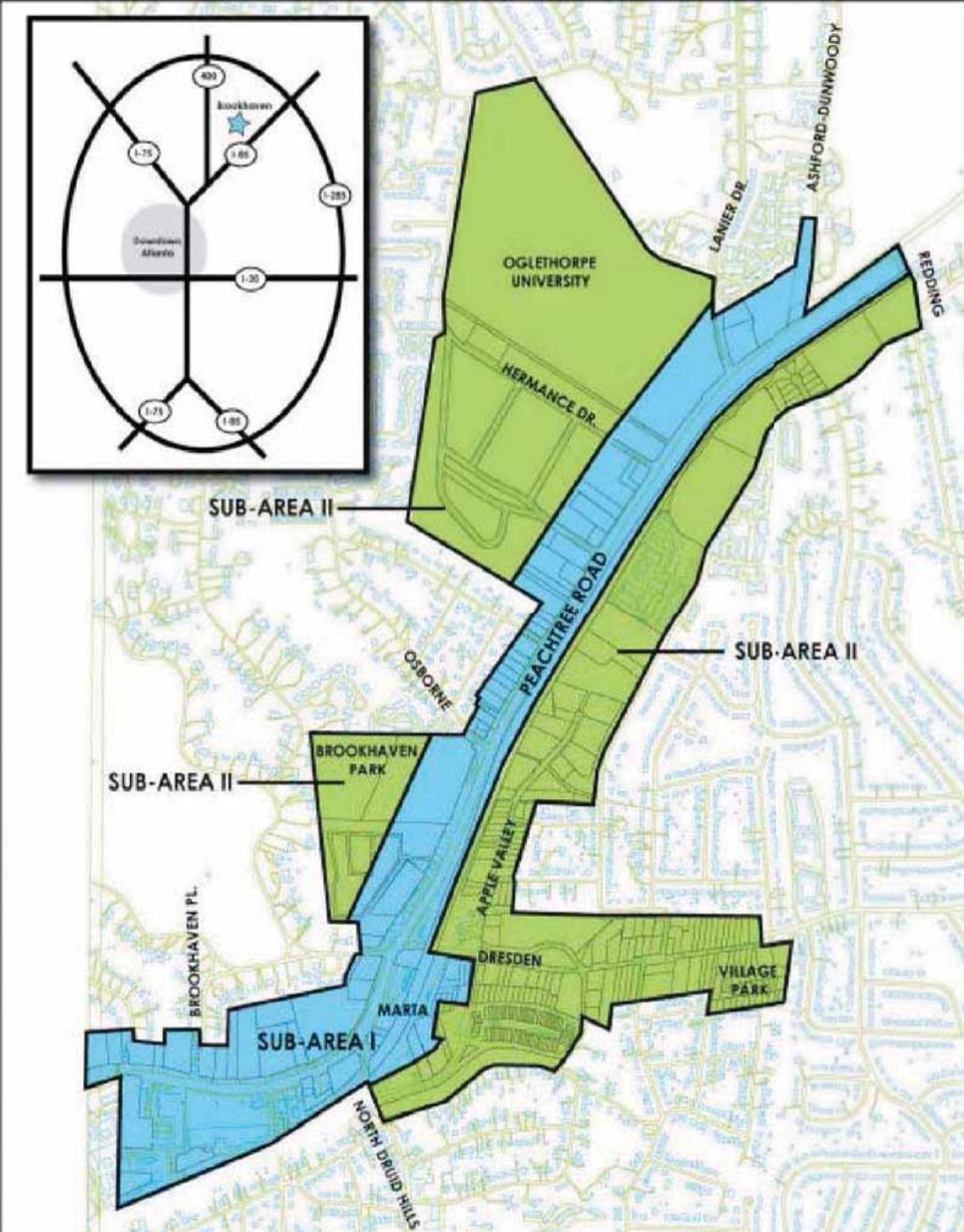
### Uses and structures

- Mixed use
- Neighborhood shopping - >50,000 ft<sup>2</sup>/tenant
- Residential
  - MF, SF, live-work
- Open space
  - 20% development parcel
- Community/rec. space
- Educational
- Office
- Medical/health
- Lodging
- Accessory uses
  - Parking decks/garages
- **TOD/MARTA station provisions**

Sub-Area I		
Building Use	Max. Building Height	Max. Building Height with Bonuses
Single-Family Attached	3 stories or 45 feet*	4 stories or 55 feet*
Commercial	6 stories or 80 feet*	7 stories or 90 feet*
Multi-family	6 stories or 80 feet*	8 stories or 100 feet*
Mixed-Use	6 stories or 80 feet*	8 stories or 100 feet*

Sub-Area II		
Building Use	Max. Building Height	Max. Building Height with Bonuses
Single-Family Attached	3 stories or 45 feet*	4 stories or 55 feet*
Commercial	4 stories or 60 feet*	5 stories or 70 feet*
Multi-family	4 stories or 60 feet*	6 stories or 80 feet*
Mixed-Use	4 stories or 60 feet*	6 stories or 80 feet*

\* whichever is less



# Brookhaven-Peachtree Overlay

## Amendments

- Building heights
  - Measured from avg. elevation of sidewalk along façade facing highest traffic vol. to top of cornice of parapet wall for flat roof or bottom of eave for sloped
  - 2<sup>nd</sup> story encompass 100% of 1<sup>st</sup> floor area; 25% may be use for architectural features
  - 2<sup>nd</sup> floor façade extend entire width 1<sup>st</sup> floor
  - Entire area of 2<sup>nd</sup> floor shall be conditioned space
- Parking - no maximums
  - Dwellings
    - SF: 1 space min.
    - MF: 1/unit min
    - Commercial: 1/400 ft<sup>2</sup>
    - Retail: 1/300 ft<sup>2</sup>
    - Restaurants: 1/125 ft<sup>2</sup> dining space
- In the absence of a provision in the Overlay, underlying zoning regulations apply



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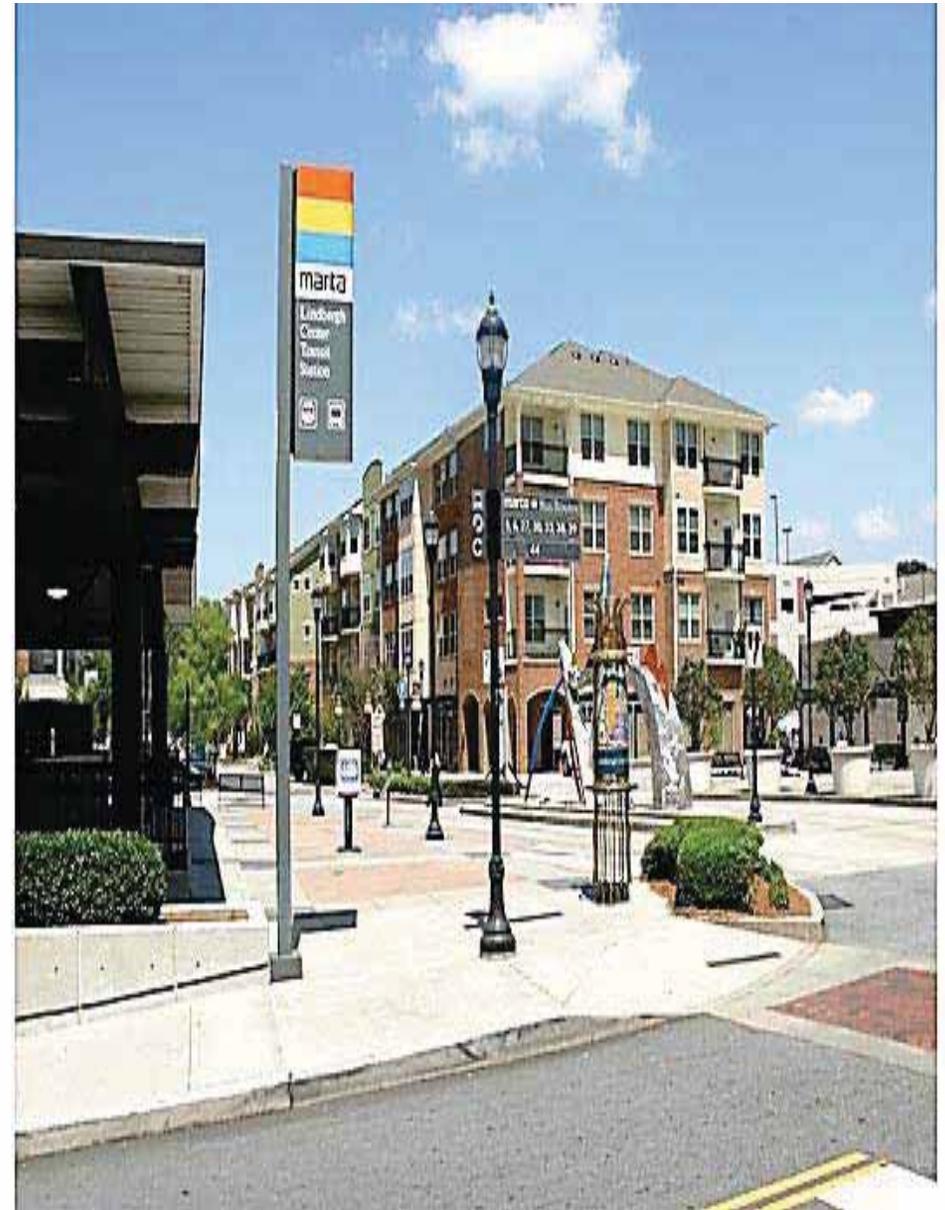
# TOD Overview

## Principles

1. Dense and compact development of station area
2. Rich mix of land uses
3. Great public space
4. New approach to parking

## Benefits

- Mobility
- Safety
- Ridership
- ↓ VMT rates
- ↑ Household income
- ↓ Air pollution and energy consumption
- Land and resource conservation
- Economic development
- ↓ Infrastructure costs



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# MARTA TOD Guidelines

- TOD STATION TYPOLOGY

- Urban Core
- Town Center
- Commuter Town Center
- Neighborhood
- Arterial Corridor
- Special Regional Destination
- Collector

Appropriate Density Ranges by Station Type			
Station Type	Floor Area Ratio (FAR)	Residential Units (per Acre)	Height (in Floors)
Urban Core	8.0-30.0	75+	8-40
Town Center or Commuter Town Center	3.0-10.0	25-75	4-15

- Mixed-use development and its usual ingredients—retail, offices, multi-family housing, civic facilities, and entertainment—are strongly encouraged.

- The standards call specifically for “vertical mixed uses”—street-level retail and upper-level offices or housing in the same buildings

- 20% residential units = affordable/workforce housing

Station Typology Matrix						
Station Type	Ideal Land Use Mix and Scale of Development	Transit Mode and Function	Public Realm	Keys to Success	Local Examples	National Examples
Urban Core	<ul style="list-style-type: none"> <li>– Downtown-scale mix of employment (office), institutional, hotel and civic uses. Return of multi-family residential is a growing trend.</li> <li>– Retail and restaurant sector gaining.</li> <li>– High-rise towers common; new buildings at least mid-rise.</li> </ul>	<ul style="list-style-type: none"> <li>– Heavy rail/ multi-modal.</li> <li>– High-volume transfers between corridors; modes</li> <li>– No park-and-ride.</li> <li>– A regional transit destination at or near system core.</li> </ul>	<ul style="list-style-type: none"> <li>– Stations usually grade-separated and closely spaced for walking.</li> <li>– Station is part of the core pedestrian network.</li> <li>– Buses stop at sidewalk.</li> </ul>	<ul style="list-style-type: none"> <li>– Attract a 24/7 mix (i.e., more residential, retail, dining, cultural).</li> <li>– Ensure station-area safety during non-9-5 hours.</li> </ul>	<ul style="list-style-type: none"> <li>– Downtown (Five Points, Peachtree, Civic Center, Garnett)</li> <li>– Midtown (North Avenue, Midtown, Arts Center)</li> </ul>	<ul style="list-style-type: none"> <li>– South Boston Waterfront (Boston, MA)</li> <li>– Market Street, San Francisco</li> </ul>
Town Center	<ul style="list-style-type: none"> <li>– Balanced mix of multi-family residential development with office, retail, entertainment, and civic uses. Vertical mixed-use is common.</li> <li>– May be pre-existing or new town center. Transition to lower-density outside the quarter-mile radius.</li> <li>– Mid-rise buildings dominate; some high- and low-rise.</li> </ul>	<ul style="list-style-type: none"> <li>– Multi-modal rail or BRT station with regional and local bus service.</li> <li>– Park-and-ride, if any, is secondary.</li> <li>– A transit origin and destination.</li> </ul>	<ul style="list-style-type: none"> <li>– Stations grade-separated (heavy rail) or at-grade.</li> <li>– Traditional town center pedestrian network with station at focal point.</li> <li>– Curb-side parking desirable; no off-street parking in front of buildings; garages wrapped.</li> </ul>	<ul style="list-style-type: none"> <li>– Get market to accept reduced residential and commercial parking.</li> <li>– Optimize street level relationships among transit, public realm, development.</li> </ul>	<ul style="list-style-type: none"> <li>– Decatur (e.g. of historic town center)</li> <li>– Brookhaven (e.g. of new town center based on LCI study)</li> </ul>	<ul style="list-style-type: none"> <li>– Rockville Town Center (Rockville, MD)</li> <li>– Mockingbird Station (Dallas, TX)</li> </ul>

# MARTA TOD-Use Standards

## Prohibited Throughout a TOD District

- Automotive sales, rental, washing, or storage
- Equipment sale, rental, or repair
- Industrial, warehousing, or distribution activities
- Construction, salvage, or junk yards
- Strip commercial development (retail in excess of 50,000 square feet in detached one- or two-story structures with surface parking in front)
- Commercial parking facilities (surface lots)
- Self- or mini-storage
- Low-density housing (under 15 units per acre)

## Prohibited in the Core Area, by Special Permit Elsewhere in a TOD District

- New single-family homes (in developments of at least 15 units per acre)
- Gas stations
- Drive-through facilities
- Commercial parking facilities (garages)

## By Special Permit Throughout a TOD District

- Retail uses in excess of 20,000 square feet per tenancy
- Retail of any area as a single use in a detached one- or two-story structure
- Cinemas as a single use in a detached one- or two-story structure
- Hotels of more than 250 rooms or suites
- Hospitals
- Laboratories or research facilities

## Allowed Uses in a TOD District

- Mixed uses, whether horizontal (adjoining uses in a single project) or vertical (different uses within the same building)
- Retail and restaurant uses of less than 20,000 square feet per tenancy in a mixed-use development or as part of an attached retail block
- Banks
- Offices
- Child care centers
- Multi-family and attached residential
- Live-work units
- Theaters, entertainment and cultural uses
- Schools and libraries
- Civic and community meeting facilities
- Cinemas in a mixed-use development
- Bed and breakfast facilities and hotels of under 250 rooms or suites
- Public open space and private open space to which the public is generally admitted.



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# MARTA TOD-Transit/Station Area Elements

- Transit Elements
- Station Area
- Street Front Experience
- Sustainability and Green Building
- Connectivity
- Overall Organization
- Parking

## DESIGN STANDARDS FOR PUBLIC REALM

### Pedestrian Zones

Minimum pedestrian clear zone on main streets	10 to 12 feet
Minimum pedestrian clear zone on mixed use or residential streets	8 to 10 feet
Minimum planting/furniture zone	6 feet

### Building/Street Front Relationship

Minimum front building set back	0 feet
Height limit for ground floor uses	16 to 20 feet
Minimum percentage of transparent surface on streetfront retail facade	50 to 60%
Minimum percentage of transparent surface on streetfront residential facade	30 to 35%

### Streets

Maximum on-street parking zone	7 to 8 feet
Minimum bike lane	5 feet
Maximum travel lane on main streets	11 feet
Maximum travel lane on mixed use or residential streets	12 feet
Maximum new block size	400 by 400 feet



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# Atlanta WalkUPs-Key Findings

Metro ATL - “poster child of sprawl”, now experiencing the end

- 27 established WalkUPs = 0.55% total land area
  - Total area = 0.88% region
  - 19% of jobs
  - 74% in City of Atlanta
  - 16/27 have rail transit
  - Avg. rent 112% higher

Since 2009...

- 50% of development
- 88% of income producing Multifamily rental real estate
- 50% of office space
- Retail lagging = 2% since 2009

## U.S. Metropolitan Land Use Options

	REGIONALLY SIGNIFICANT	LOCAL SERVING
 WALKABLE URBAN	<b>WALKUP</b> (Walkable Urban Place) <i>1-2% of Metro Area Acreage</i>	<b>NEIGHBORHOOD</b> <i>3-7% of Metro Area Acreage</i>
 DRIVABLE SUB-URBAN	<b>EDGE CITY</b> <i>5-7% of Metro Area Acreage</i>	<b>BEDROOM COMMUNITY</b> <i>80-85% of Metro Area Acreage</i>



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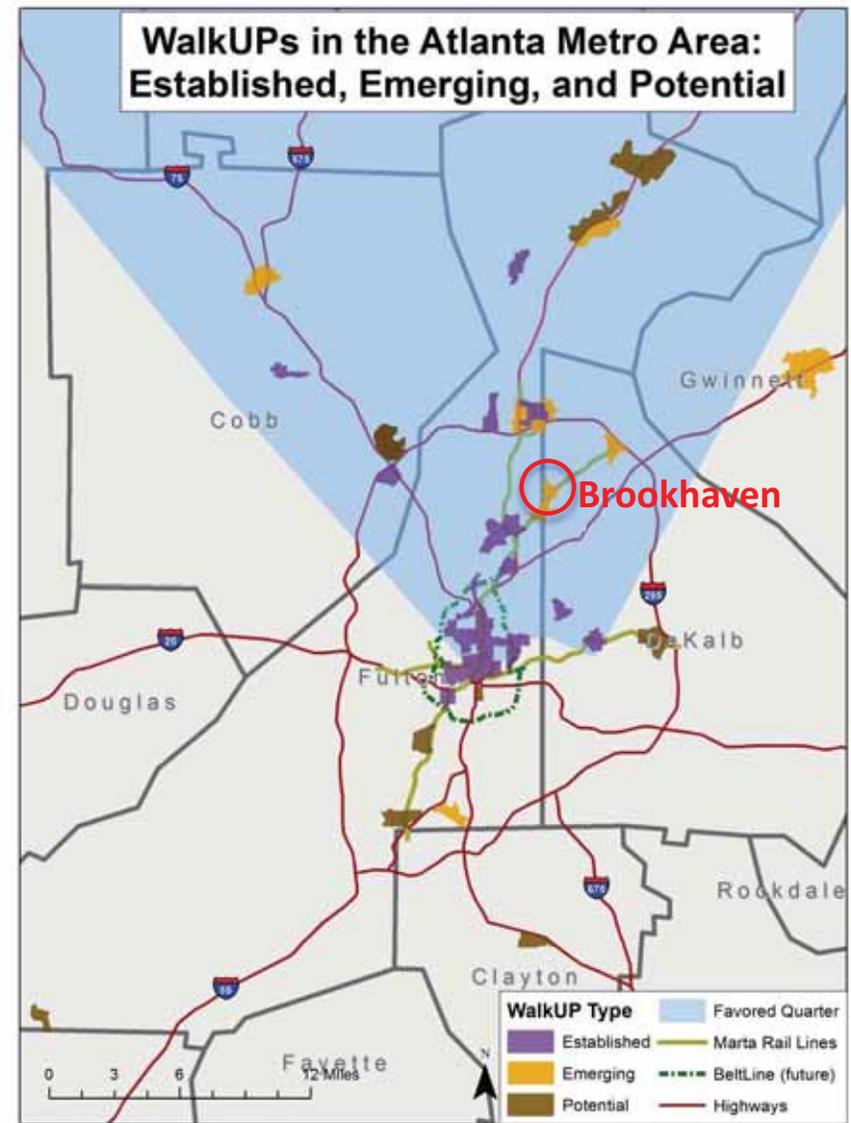
# Atlanta WalkUPs-Conclusions

## Economic

- 70% of variation in economic performance explained by:
  1. Educational attainment
  2. Share of jobs concentrated in knowledge industries

## Social Equity

- Strong economic performance associated with lower measures of social equity
  - Exceptions = Midtown and Downtown Decatur



# BROOKHAVEN-OGLETHORPE MARTA STATION CHARRETTE

BACKGROUND SESSION 2: TRANSPORTATION PLANNING  
OCTOBER 14, 2013



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# Background Session Agenda

- I. Welcome & Introductions
- II. Flyer/Schedule
- III. Charrette Overview
- IV. Streetscape Design Examples
- V. Traffic Data
- VI. Low-Speed Vehicles
- VII. Questions & Answers



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# Project Team

## Southface

- Robert Reed, Alex Trachtenberg, Gray Kelly, Clay Elliot, Maggie Baynham, Dustin Norton and David Knorr

## City of Brookhaven

- Mayor J. Max Davis, Rebecca Chase Williams, Bates Mattison, Jim Eyre, Joe Gebbia, Marie Garrett, Susan Canon

## Atlanta Regional Commission

- Dan Reuter, Jared Lombard and Allison Duncan

## Atlanta/National Board of Realtors

- Lennie Shewmaker

## MARTA

- John Crocker and Jason Ward

## Design Team

- Eric Bosman, Bob Munger, Eric Parker, Clarence Vinson, Bryan Long, Michael Roberts, Christina Davis, Dave Skelton, Dwayne Tedder, Glenn Kurtz, Jason Frost, Bill Bassett, Joe Palladi, Mike Lobdell, Joel Putterman, Paul Moore, John Ladson, Jay Scott, John Funny, Laurel Hart, Joe Collums, Jack Honderd, Mike Elliot, Art Lomenick

# Brookhaven-Oglethorpe MARTA Station Transit Oriented Development Charrette

All sessions held at Oglethorpe Presbyterian Church, 3016 Lanier Drive Atlanta, GA 30319



## Background Sessions

**Monday, October 7**  
6:30-8:30 PM

- ◆ "Those who cannot remember the past are condemned to repeat it": Understanding the Brookhaven-Peachtree Overlay and LCI Study

**Monday, October 14**  
6:30-8:30 PM

- ◆ Transportation Planning: Putting the Pieces Together for a Multi-Modal Framework

**Thursday, October 17**  
6:30-8:30 PM

**Mellow Mushroom-Brookhaven**  
4058 Peachtree Rd NE Atlanta, GA 30319

- ◆ Workforce Housing: Good for the Community and Those Who Serve It

## Charrette Event

**Sunday, October 20 - Thursday, October 24**

**Sunday Evening**  
6:30-8:30 PM

- ◆ Community Meeting-Charrette Kickoff

**Monday - Thursday**  
8:00 AM - 5:00 PM

- ◆ Stakeholders meeting times TBD

**Thursday Evening**  
6:30-8:30 PM

- ◆ Community Meeting  
Presentation of Deliverables



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# Charrette Schedule

Brookhaven-Oglethorpe MARTA Charrette	Sunday, October 20	Monday, October 21	Tuesday, October 22	Wednesday, October 23	Thursday, October 24	COLOR KEY	DELIVERABLE		
8:00 AM				Work on MARTA Station Area Neighborhoods Engagement Continues			MARTA Station Area Neighborhoods		
8:30 AM							Opening/Closing Public Meeting		
9:00 AM									
9:30 AM									
10:00 AM									
10:30 AM									
11:00 AM									
11:30 AM									
12:00 PM									
12:30 PM									
1:00 PM									
1:30 PM									
2:00 PM									
2:30 PM									
3:00 PM									
3:30 PM									
4:00 PM									
4:30 PM		MARTA Station Area Neighborhoods Engagement							
5:00 PM				MARTA Station Area Neighborhoods Engagement					
5:30 PM									
6:00 PM									
6:30 PM	Public Kickoff Meeting								
7:00 PM									
7:30 PM									
8:00 PM									
8:30 PM									
9:00 PM									
LOCATION	<p>All meetings will take place at <b>Oglethorpe Presbyterian Church - 3016 Lanier Dr. Atlanta, GA 30319</b></p> <p>Neighborhood Engagement sessions will take place in the Library, Parlor and Gathering Room - Southface interns will direct stakeholders to their appropriate room.</p> <p><b>Public meetings will take place in the Sanctuary on Sunday, October 20th 6:30-8:30 PM and Thursday, October 24th 6:30-8:30 PM</b></p> <p>Contact: Alex Trachtenberg - Southface 404-604-3592 atrachtenberg@southface.org</p>								

# Charrette Overview



## Edgewood-Candler Park MARTA Station Charrette

- Adaptive, reactive, efficient and engaging on multiple levels
- Open process including all interested parties to produce a feasible plan w/ minimal additional work

### Timeframe:

- Research, education and Charrette preparation = 1-9 mo.
- Charrette - 4 days
  - Public meeting held evening prior to 1<sup>st</sup> day of Charrette
- Deliverables = 2-4 mo. post Charrette
  - Report
  - Additional deliverables



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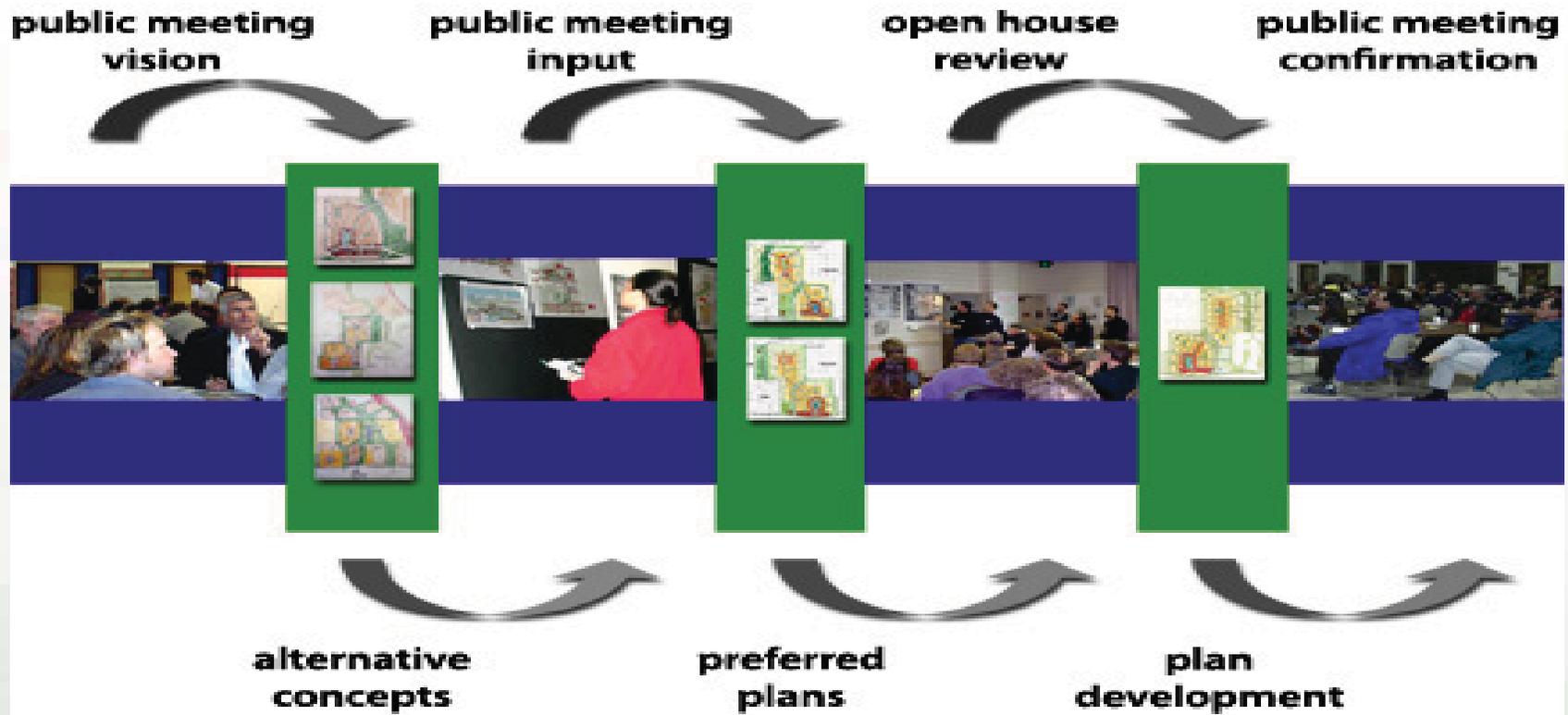


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# Charrette Event



- Project Team develops plan
- Stakeholders review
  - Stakeholders attend feedback meetings at critical decision making points
    - Stakeholders are not in attendance all the time
- Directly affected groups (citizens, business owners, etc.) are involved in short feedback loops or meetings

# Charrette Process

## Project Team

- Deliverables Description
- Guiding Principles
- Stakeholder Outreach
- Base Data Research & Analysis

- Pre-Charrette Brief and Charrette
- Values, Vision and Needs
- Alternatives

- Deliverables

June 2013

October 2013

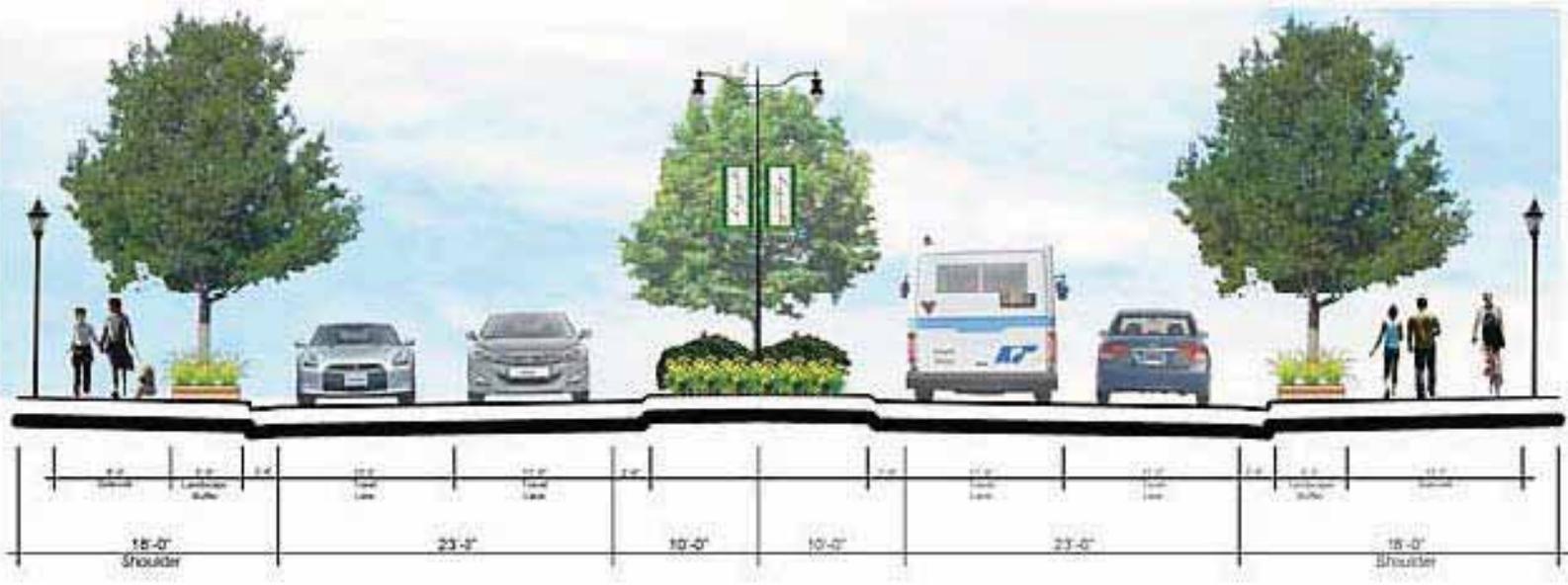
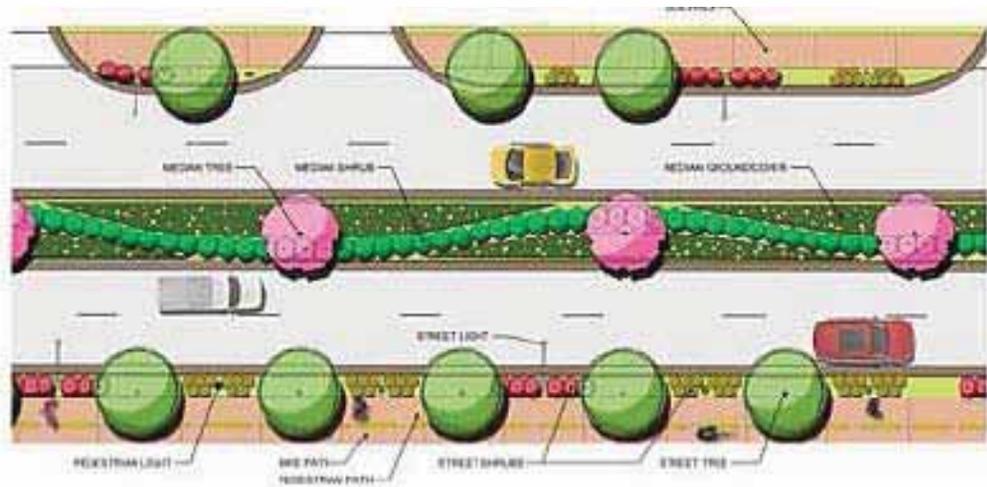
January 2014

## Stakeholders

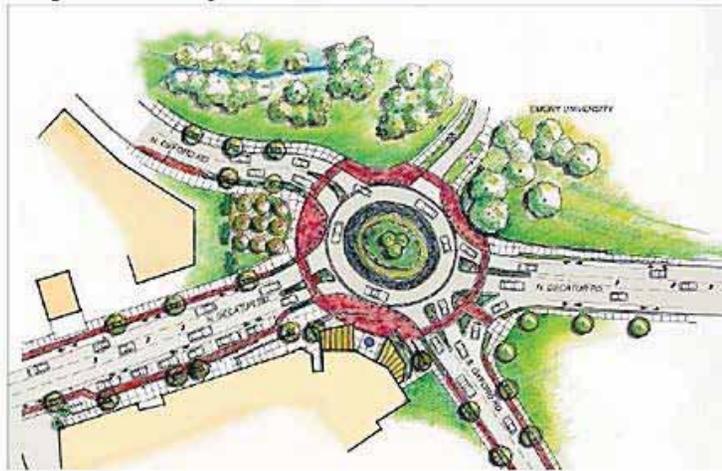
- Kickoff Meeting
- Stakeholder Interviews
- Educational Events (3)
- Charrette
- Meetings (3)
- Open Public Meeting
- Alternatives Public Meeting
- Final Public Meeting

- Communication and Follow-Up Meeting

# 15<sup>th</sup> Street-Augusta, GA



# Emory Village



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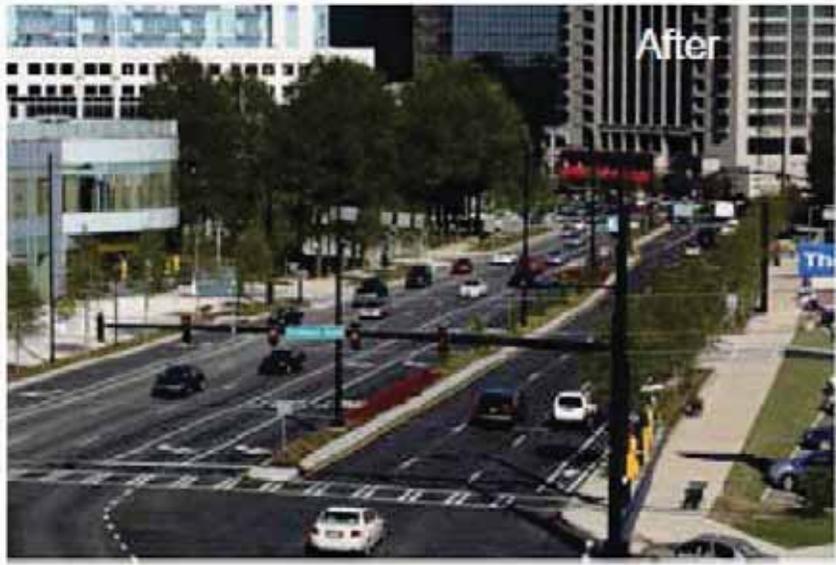


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# Peachtree Road-Buckhead



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# Boulevard Design-Local Access Lane



# Boulevard Design-Thru Lanes



# MARTA Station-Traffic Safety

Bookmarks

Search

By Area

By Location

Location:

Street Address:

Zip Code:

1. 4047 Peachtree Road, 30319  
 Longitude: -84.3397  
 Latitude: 33.8606

By GDOT Data

Query Tools

Layers

Bridges

Crashes

**Crashes (Last 3 Years)**

**161 Crashes found.**

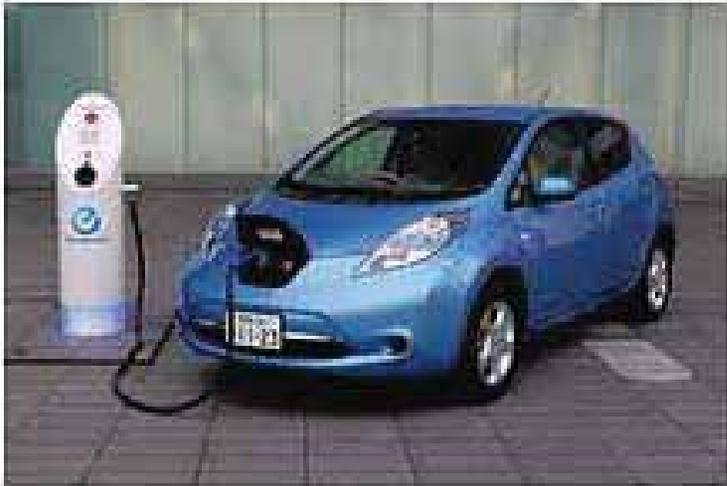
Accident ID	Accident Date
143124	1/5/2009 6:07:00 F
148785	1/11/2009 2:10:00
151557	1/13/2009 7:30:00
156853	1/30/2009 7:30:00
173743	2/27/2009 8:45:00
1766700	1/15/2010 8:20:00
1768794	1/17/2010 10:17:0
1779087	11/30/2009 5:30:0
1790975	12/4/2009 6:24:00
191017	3/15/2009 1:48:00
194178	3/18/2009 11:30:0
205778	3/27/2009 1:46:00

Permits

Projects

Traffic

# Sustainable Transportation



# Low-Speed Vehicles-Georgia

## Georgia is a Global Leader in Production of Green, Low Speed EV Transportation

Club Car, E-Z GO, Yamaha and Tomberlin are all headquartered in Georgia

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# Low-Speed Vehicles

## What is an LSV?

- According to 1998 NHTSA classification, a Low Speed Vehicle has:
  - 4 wheels
  - Top speed of 20-25 MPH on level ground
  - Weight less than 3000 lbs. (most under 2000 lbs.)
  - Compliance with National Highway Safety Administration regulations (golf carts do not comply) for LSV's. Includes lights, brakes, signals, seat belts, roll bars, etc.
  - A Vehicle Identification Number
  - Street legal in 46 states on roads 35 MPH or less.



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# Personal Transport Vehicle

## What is a PTV (Personal Transport Vehicle)?

- 4 wheels
- Top speed of 20 MPH or less
- Weight less than 1375 lbs.
- Has brakes, head lamps, hip restraints, tail lamps, reverse warning, horn, rear view mirror
- Street legal on low speed roads in Georgia with local PTV ordinance governing their use



As of 2012, Georgia SB240 allows PTV's to be driven on local roads, upon adoption of a local ordinance governing their use.

Many PTV's are converted golf cars

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# LSV/PTV Infrastructure

## Different Infrastructure for Different Types of Low Speed Vehicles

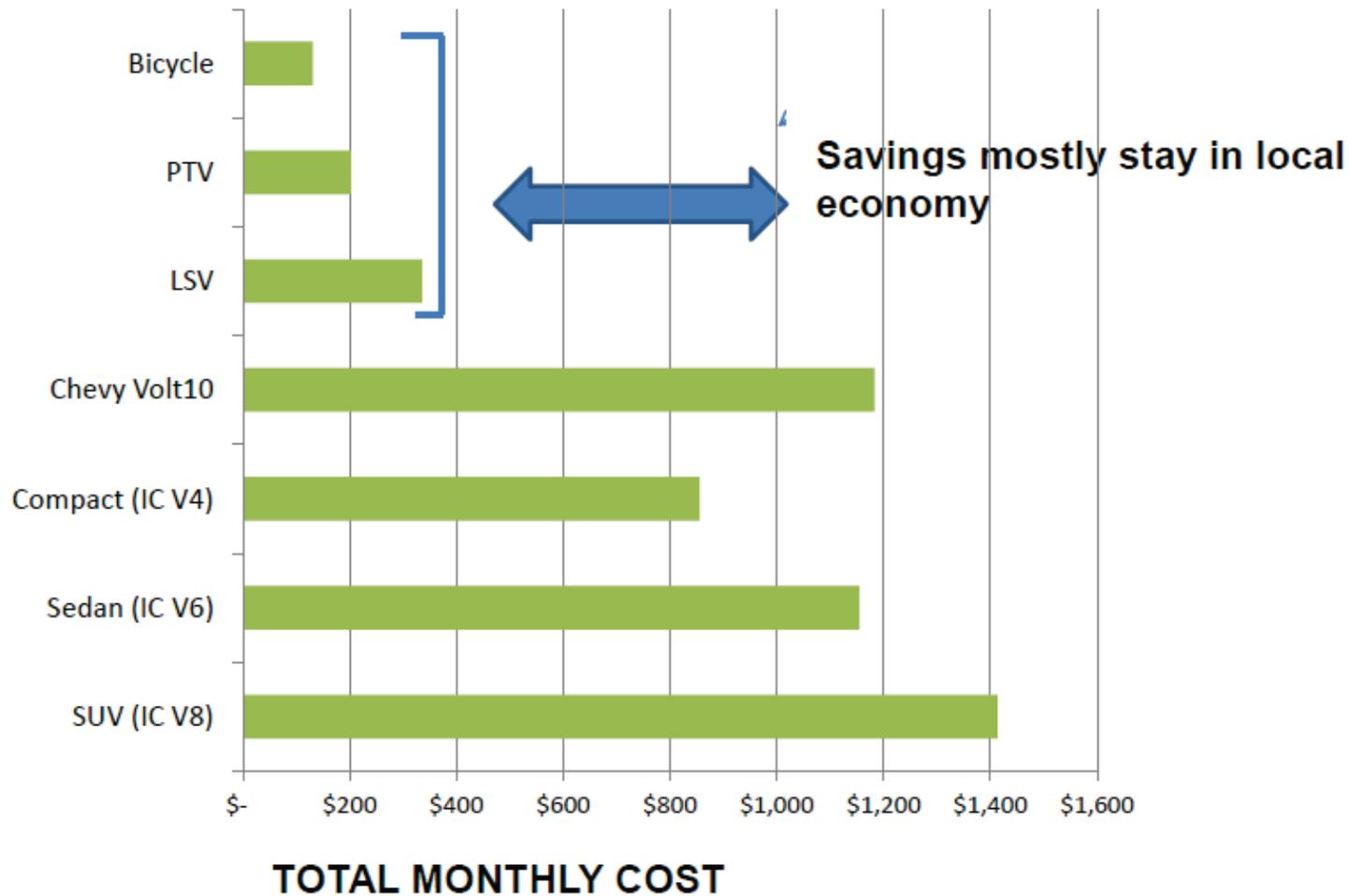
- LSV's are street legal and well-suited for existing low speed roads (suggest 25-30 MPH)
- PTV's can be street legal and well-suited for very low speed roads (20-25 MPH)
- Golf cars are for multi-purpose trails

An Alternative Transport Corridor that was wide enough could accommodate all three, along with bicycles.



# Transportation Costs

## Comparison of Transportation Cost



Total cost includes assumption of \$3.50/gallon retail gas price and 1000 miles per month travel

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# BROOKHAVEN-OGLETHORPE MARTA STATION CHARRETTE

BACKGROUND SESSION 3: WORKFORCE HOUSING  
OCTOBER 17, 2013



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# Background Session Agenda

- I. Welcome & Introductions
- II. Flyer/Schedule
- III. Charrette Overview
- IV. Workforce Housing Overview
- V. Columbia Residential Example
- VI. Questions & Answers



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# Project Team

## Southface

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# Brookhaven-Oglethorpe MARTA Station Transit Oriented Development Charrette

All sessions held at Oglethorpe Presbyterian Church, 3016 Lanier Drive Atlanta, GA 30319



## Background Sessions

**Monday, October 7**  
6:30-8:30 PM

- ◆ "Those who cannot remember the past are condemned to repeat it": Understanding the Brookhaven-Peachtree Overlay and LCI Study

**Monday, October 14**  
6:30-8:30 PM

- ◆ Transportation Planning: Putting the Pieces Together for a Multi-Modal Framework

**Thursday, October 17**  
6:30-8:30 PM

- Mellow Mushroom-Brookhaven**  
4058 Peachtree Rd NE Atlanta, GA 30319
- ◆ Workforce Housing: Good for the Community and Those Who Serve It

## Charrette Event

**Sunday, October 20 - Thursday, October 24**

**Sunday Evening**  
6:30-8:30 PM

- ◆ Community Meeting-Charrette Kickoff

**Monday - Thursday**  
8:00 AM - 5:00 PM

- ◆ Stakeholders meeting times TBD

**Thursday Evening**  
6:30-8:30 PM

- ◆ Community Meeting  
Presentation of Deliverables



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# Charrette Schedule-Public Meetings

## Engagement Opportunities for City of Brookhaven Citizens

- All meetings are at Oglethorpe Presbyterian Church - 3016 Lanier Dr. 30319
- During the Charrette anyone is welcome to drop by and review progress to date and provide input. However the times below are available for a more detailed presentation and input.

Interest Area	Date	Time
General Public Meeting	Sunday, Oct 20 <sup>th</sup>	6:30 pm
MARTA Station Area –Bounding Properties development input	Monday, Oct 21 <sup>st</sup>	4pm
MARTA Station Area –Bounding Properties development input Additional opportunity for input	Tuesday, Oct 22 <sup>nd</sup>	8am
MARTA Development Quality Guidelines-input opportunity	Tuesday, Oct 22 <sup>nd</sup>	2pm
Fernwood Park Stormwater Facility Review	Tuesday, Oct 22 <sup>nd</sup>	6pm
Brookhaven Connectivity and Low Speed Vehicle-Preliminary Review	Tuesday, Oct 22 <sup>nd</sup>	4pm
Streetscape design review	Wednesday Oct 23 <sup>rd</sup>	10am
Station Area Neighborhood- Recommendations Review	Wednesday Oct 22 <sup>nd</sup>	5pm
Final Public Meeting	Thursday Oct 24 <sup>th</sup>	6:30pm

# Housing Tax Credits

GEORGIA DEPARTMENT  
OF COMMUNITY AFFAIRS

Joe Collums  
Senior Policy Analyst



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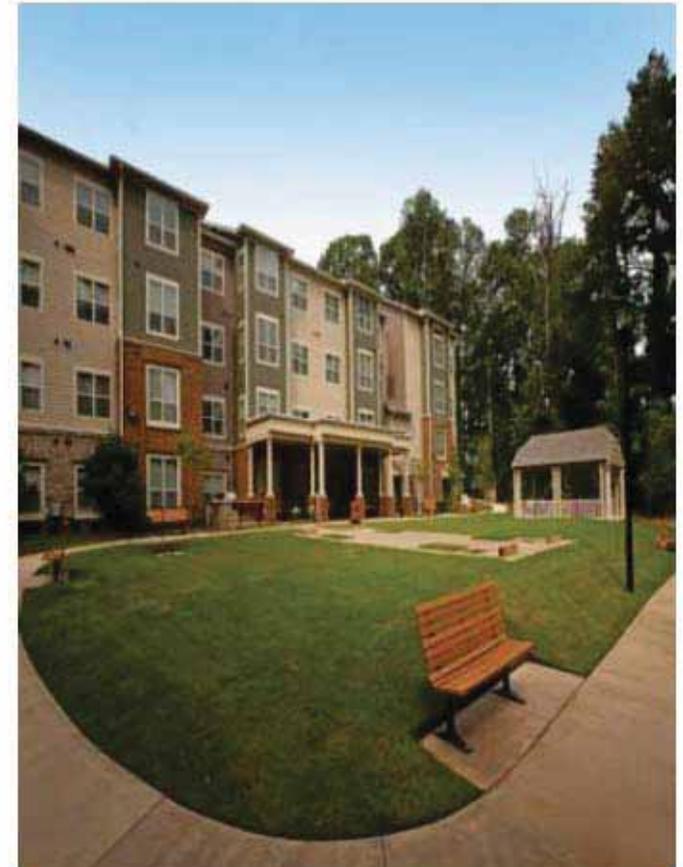
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# What is the Housing Tax Credit program?

- Enacted by Tax Reform Act of 1986
- Tax incentive program to increase supply of affordable rental housing
- Designed to attract private investment
- Targets working families and seniors



# Housing Credit Impact

Addresses critical need for safe, decent, affordable housing for:

- Seniors in an independent living environment
- Moderate income families in need of workforce housing
- Individuals with disabilities



*Residents at Lone Mountain Village,  
Ringgold GA*



# Workforce Housing Calculation

DeKalb County, Georgia										
FY 2013 Income Limit Area	Median Income <a href="#">Click Here</a>	FY 2013 Income Limit Category	1 Person	2 Person	3 Person	4 Person	5 Person	6 Person	7 Person	8 Person
DeKalb County	\$66,300	Very Low (50%) Income Limits <a href="#">Click Here</a>	\$23,250	\$26,550	\$29,850	<b>\$33,150</b>	\$35,850	\$38,500	\$41,150	\$43,800
		Extremely Low (30%) Income Limits <a href="#">Click Here</a>	\$13,950	\$15,950	\$17,950	<b>\$19,900</b>	\$21,500	\$23,100	\$24,700	\$26,300
		Low (80%) Income Limits <a href="#">Click Here</a>	\$37,150	\$42,450	\$47,750	<b>\$53,050</b>	\$57,300	\$61,550	\$65,800	\$70,050

NOTE: DeKalb County is part of the Atlanta-Sandy Springs-Marietta, GA HUD Metro FMR Area, so all information presented here applies to all of the Atlanta-Sandy Springs-Marietta, GA HUD Metro FMR Area. The Atlanta-Sandy Springs-Marietta, GA HUD Metro FMR Area contains the following areas: Barrow County, GA ; Bartow County, GA ; Carroll County, GA ; Cherokee County, GA ; Clayton County, GA ; Cobb County, GA ; Coweta County, GA ; Dawson County, GA ; DeKalb County, GA ; Douglas County, GA ; Fayette County, GA ; Forsyth County, GA ; Fulton County, GA ; Gwinnett County, GA ; Heard County, GA ; Henry County, GA ; Jasper County, GA ; Newton County, GA ; Paulding County, GA ; Pickens County, GA ; Pike County, GA ; Rockdale County, GA ; Spalding County, GA ; and Walton County, GA .

Brookhaven Area Median  
Income = \$56,231

Brookhaven Police  
Starting Salary ~ \$45,000

# Qualified Allocation Plan (QAP)



- Development schedule
- Approval process

**Thank You!**



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# Columbia Residential Development



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# Columbia Residential Development



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# Columbia Residential Development



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# Columbia Residential Development



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# Columbia Residential Development



# Columbia Residential Development



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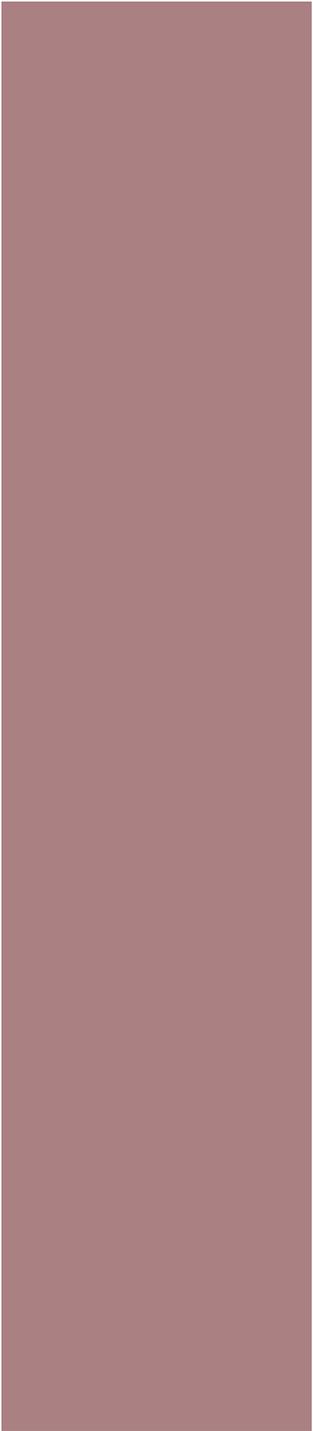


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## Appendix F: Media Coverage

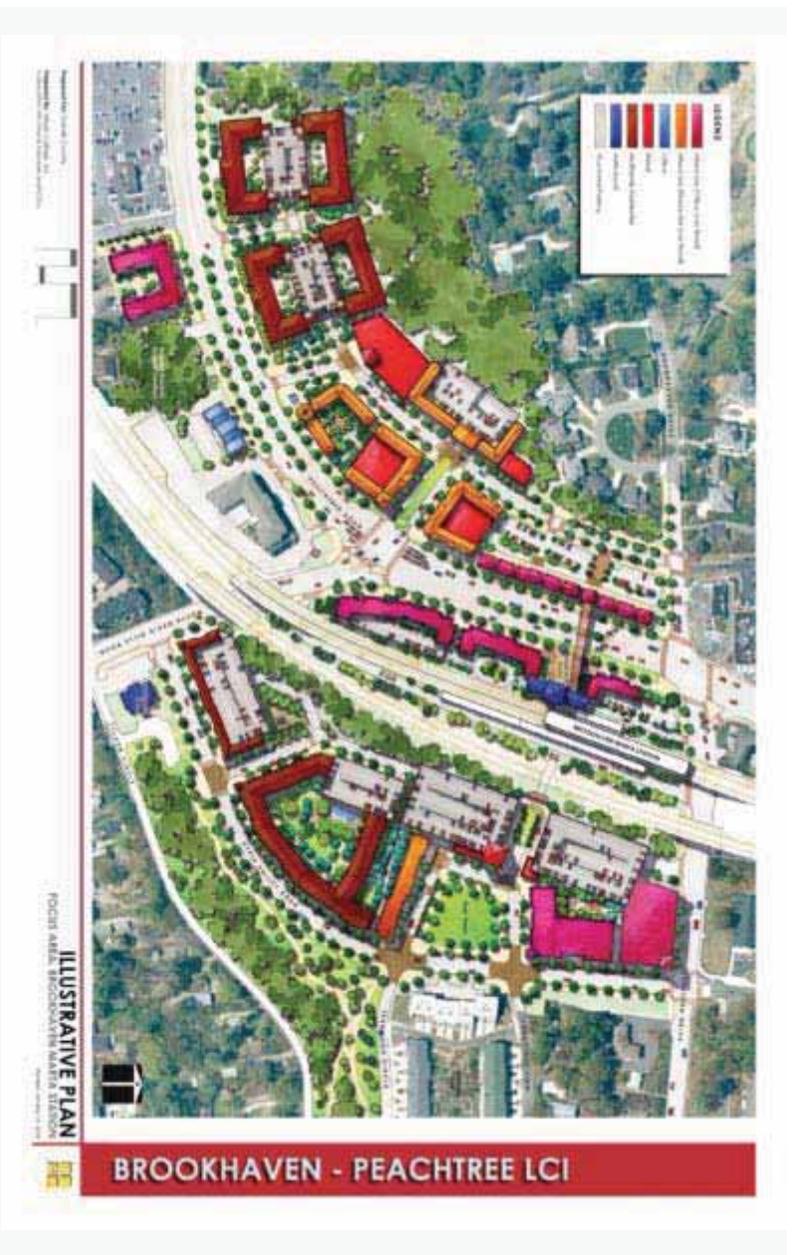


# Brookhaven-Oglethorpe MARTA Charrette Media Coverage

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**UPDATE: Brookhaven-Oglethorpe MARTA Station  
T.O.D. Charrette begins 10/7. Detailed Event schedule  
added.**

October 3, 2013 | Filed under: In and Around | Posted by: thebrookhavenpost



Brookhaven – Oglethorpe MARTA Station Transit Oriented Development Charrette

**Brookhaven, GA** – Beginning Monday, Background Sessions – part of the Brookhaven-Oglethorpe MARTA Station Transit Oriented Development Charrette – begin.

The first background session will be on the Brookhaven-Peachtree Overlay and LCI Study, October 7th, followed by Transportation Planning on October 14th, then finally, Workforce Housing on October 17th.

The actual Charrette Event, will begin on October 20th, and will run through October 24th. The agenda includes work sessions and meetings that will take a deep dive into several areas of deliverables such as:

- Traffic/Peachtree Pedestrian Design
- Historic Brookhaven Stakeholder Engagement
- Quality Development Guidelines Phasing Plan
- Stormwater Facility
- MARTA Agreement
- Brookhaven Development Authority Sub-Committee
- Connectivity/LSV Plan

### Schedule of Sessions and Events

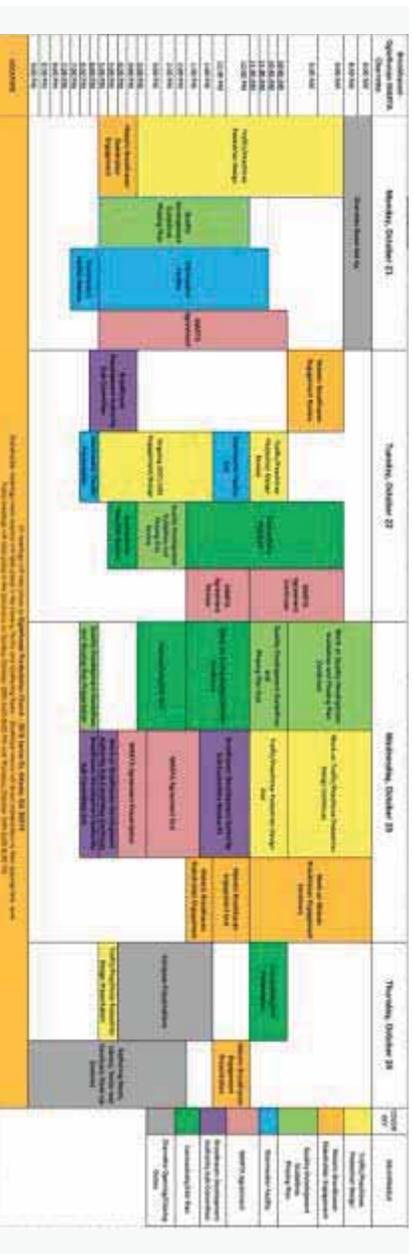
#### Background Sessions

- Monday, October 7**  
**6:30-8:30 PM**
- ♦ "Those who cannot remember the past are condemned to repeat it"; Understanding the Brookhaven-Peachtree Overlay and LCI Study
- Monday, October 14**  
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- ♦ Transportation Planning: Putting the Pieces Together for a Multi-Modal Framework
- Thursday, October 17**  
**6:30-8:30 PM**
- ♦ Mellow Mushroom-Brookhaven  
 4058 Peachtree Rd NE Atlanta, GA 30319
  - ♦ Workforce Housing: Good for the Community and Those Who Serve It

#### Charrette Event

- Sunday, October 20 - Thursday, October 24**
- Sunday Evening**  
**6:30-8:30 PM**
- ♦ Community Meeting-Charrette Kickoff
- Monday - Thursday**  
**8:00 AM - 5:00 PM**
- ♦ Stakeholders meeting times TBD
- Thursday Evening**  
**6:30-8:30 PM**
- ♦ Community Meeting
  - ♦ Presentation of Deliverables

Session Schedules



Detailed Charrette Event Schedule – Click to Download PDF

**All sessions held at Oglethorpe Presbyterian Church, 3016 Lanier Drive Atlanta, GA 30319, except 10/17, that will be at Mellow Mushroom. See above.**

### Project Background

The Charrette is an initiative of the project team comprised of the City of Brookhaven, Southface, Atlanta Regional Commission (ARC), MARTA and National/Atlanta Commercial Board of REALTORS™.

A TOD is a pedestrian-friendly, mixed-use community that provides relief from automobile-dependent lifestyles by increasing access to transit for residents and workers, enabling them to drive their cars less and ride transit more. TODs provide people with options so they can choose where to live, work, play and mingle. In the most successful TODs, transit stations integrate seamlessly into the community.

The ARC, in partnership with MARTA and others including Southface, have been working to promote TOD at each MARTA station to ensure that these transit stations will have the necessary infrastructure and land uses to support planned and future TODs. Many of the MARTA station parking lots, including the Brookhaven-Oglethorpe station, are vastly underutilized and present an opportunity for MARTA and the community to benefit from redevelopment.

The Brookhaven-Oglethorpe MARTA station has been identified through various studies and reports as being in the “advanced” stage for TOD implementation due to a strong residential real-estate market, established cityhood, need for additional office and retail space, improved ridership and the vision to create a “city center” on the property composed of mixed use, office, retail, civic and public space.

The Charrette will focus on producing deliverables geared towards implementing TOD on the MARTA site. The focus will be less on the design of the proposed development than the necessary steps for implementation due to existing plans, studies and ordinances including the Brookhaven-Peachtree LCI Study and Brookhaven-Peachtree Overlay District.

## **What is a Charrette?**

The Charrette is the transformative event of the NCI Charrette System and occurs from 6-weeks to 9-months after the completion of the Research, Education and Charrette Preparation Phase. It is a collaborative design event that lasts a minimum of 5-days. The goal of the Charrette is to produce a feasible plan with minimal rework that benefits from the support of all stakeholders through its implementation. This support is facilitated by the ability of the Charrette to transform the mindsets of the stakeholders. (See blog discussion on Charrette for sustainable community design.)

A multidisciplinary Charrette team, consisting of consultants and sponsor staff, produces the plan. Stakeholders – those being anyone who can approve, promote or block the project as well as anyone directly affected by the outcomes – are involved through a series of short feedback loops or meetings. Most stakeholders attend two or three feedback meetings at critical decision-making points during the Charrette.

<http://brookhavenpost.co/update-brookhaven-oglethorpe-marta-station-to-d-charrette-begins-107-detailed-event-schedule-added/9522>

## **Possible face lift for Brookhaven-Oglethorpe MARTA station**

By Sarah Anne Voyles

[svoyles@neighbornewspaper.com](mailto:svoyles@neighbornewspaper.com)

October 15, 2013 09:52 AM | 1081 views | 0  | 16  |  | 

MARTA is looking into the possibility of redeveloping the Brookhaven-Oglethorpe MARTA station.

The transit authority spokesman Lyle Harris said the transit authority will have three different roles for its involvement with the redevelopment of the property — property owner, transit provider and community stakeholder.

“Our role as the property owner is to ensure that a fair, market rate of return is achieved, as required by our enabling legislation and Federal Transit Administration guidelines,” Harris said. “Our role as a transit provider is to ensure that our transit customers who arrive by bus, rail, bicycle, foot and other forms of transportation are accommodated in the future development and our role as a stakeholder is to promote sustainable design and construction techniques, help facilitate the inclusion of workforce housing in the future development and help facilitate increased connectivity and access to and from the station and the surrounding neighborhood.”

For the project, MARTA will be looking into different partnerships with the city of Brookhaven and several regional partners including the Atlanta Regional Commission and Southface. Southface is planning a charrette later this month as an opportunity for different developers to join in with plans.

Brookhaven released a statement from the Community Development Director Susan Canon stating the city is not planning any redevelopment in the area surrounding the station.

“The city of Brookhaven currently has no plans for redevelopment around the MARTA station,” Canon said. “We are preparing to launch our own redevelopment plan for the city with the comprehensive plan. This will include the entire city, including the area around the MARTA station. We have issued an RFP for a consultant to lead the comprehensive plan and had a pre-bid conference. Once this plan is complete, we will have information about future development for the MARTA station area.

As the property owner, MARTA’s board of directors will have the final say in any redevelopment plans for the station.

[http://www.neighbornewspapers.com/view/full\\_story/23855183/article-Possible-face-lift-for-Brookhaven-Oglethorpe-MARTA-station?instance=brookhaven](http://www.neighbornewspapers.com/view/full_story/23855183/article-Possible-face-lift-for-Brookhaven-Oglethorpe-MARTA-station?instance=brookhaven)

Posted by [Melissa Weirman](#) on October 18, 2013.

## Improving Peachtree Road is important for MARTA redevelopment

Peachtree Road is one of Brookhaven's busiest streets, but some are hopeful that the thoroughfare may one day provide better access to people traveling by bike or by foot.

At background sessions leading up to a four-day charrette, Brookhaven residents began sharing their vision for the redevelopment of the Brookhaven-Oglethorpe MARTA station.

The planning and brainstorming event, known as a charrette, was scheduled for Oct. 21-24. The charrette brings together architects, urban planners, transportation experts and stakeholders to discuss ways to make "transit-oriented development" at the MARTA station a reality.

The Brookhaven-Oglethorpe MARTA station is one of several stations around Atlanta with underutilized parking lots that have been identified as strong candidates for redevelopment with a mix of residential and commercial uses.

Robert Reed, communities design director with Southface, an environmental nonprofit organizing the events, said the purpose of the charrette is to help the community zero in on what it wants before a developer is selected for the project.

"One of our deliverables is a quality development guideline," Reed said.

The Brookhaven community has long been interested in focusing more resources around the MARTA station, located at the intersection of Peachtree and North Druid Hills roads.

A Livable Centers Initiative study conducted by the Atlanta Regional Commission in 2006 identified the MARTA station as a centerpiece for a future Brookhaven town center that would include a mix of office, retail and public spaces.

But Reed said people must be able to safely walk to and from the MARTA station before a transit-oriented development can be successful.

"We have to have a pedestrian-friendly Peachtree Street," Reed said.

The goal is to improve safety and usability along Peachtree for pedestrians, bikers, and other alternative modes of transportation.

"Everyone complains about car traffic, but there's more than one way to get around," Reed said.

Some residents voiced concerns about traffic along Peachtree. They said the road is already congested without future development around MARTA and the new apartment buildings under construction along Dresden Drive.

Reed said making roads friendlier for alternative transportation lessens the impact of development, but won't solve current traffic issues.

“I’m not saying this development is going to make it better, but this development is going to happen one way or another,” Reed said.

Bob Munger, president of the Augusta Greenway Alliance, shared information about low-speed vehicles. His organization promotes sustainable transportation, including the use of golf-cart-like, low-speed vehicles, which he said are an environmentally-friendly option.

He said the vehicles can be used on roads with low speed limits and on multi-purpose trails. Unlike electric cars, he said they can be charged at home without a special charging station at a cost of about 2 cents per mile.

“We emphasize it because we feel we have an excellent way to get around that’s underutilized,” Munger said. “The vehicles are very economical to own and operate.”

Dan Reuter, with the Atlanta Regional Commission, said more dense, urban development is the trend in the region.

“Our region is going to continue to grow,” Reuter said. “People are moving back into urban places, particularly young people.”

Rent is higher in walkable, urban areas, which benefits local economies, he said.

“These urban places have really been run up in the past five to 10 years. They’re very desirable places to live,” Reuter said.

<http://www.reporternewspapers.net/2013/10/18/improving-peachtree-road--important-marta-redevelopment-2/>

# Brookhaven MARTA Station Meetings Continue

Public invited to learn about development around station.

Posted by [Ann Marie Quill](#) (Editor), October 22, 2013 at 04:39 PM

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As the city of [Brookhaven's Community Development Plan](#) is under way, Southface Energy Institute is also hosting a series of charrettes concerning development around the Brookhaven MARTA station.

These meetings are open to the public and not city-sponsored.

- **Tuesday, Oct. 22, 6 p.m.:** Fernwood Park Stormwater Facility Review
- **Wednesday, Oct. 23, 10 a.m.:** Streetscape Design Review
- **Wednesday, Oct. 23, 5 p.m.:** Station Area Neighborhood- Recommendations Review
- **Thursday, Oct. 24, 6:30 p.m.:** Final Public Meeting

<http://brookhaven.patch.com/groups/politics-and-elections/p/brookhaven-transit-development-meetings-conclude-this-week>

## **MARTA Charrette Discussion**

**Meeting** 10/22/2013 4:30 PM

**October 22, 2013 4:30 PM**

Council members Gebbia and Eyre offered comments about a proposal to include the findings of the MARTA Station TOD Charrette, as well as a more detailed analysis of the station, as a chapter in the Comprehensive Land Use Plan. Ms. Garrett pointed out that one of the most important components of the MARTA TOD plan was to develop an established, identified, and committed relationship with MARTA. She offered further comments on the importance of a strong relationship with MARTA and the implications to the city of the station's future development. Council member Gebbia expressed support for the proposal and claimed that the outcome of the MARTA Station TOD would be a defining statement for the city. He expressed a desire to develop a strong relationship with MARTA, and expressed concerns that the expectations for the MARTA development were too low. He encouraged the city to complement the work of the TOD Charrette and combine the best of both ideas into the master plan. Ms. Garrett pointed out that a critical component of the proposal would be for both sides to reach an agreement on the station's future development plans. She pointed out that at the end of the day, it was the responsibility of the mayor and council to make sure that the city could afford service delivery for any new developments; it was not MARTA's responsibility to provide said services.

Council member Williams stated that she and council member Mattison had just attended a Charrette meeting and offered comments about how the Charrette was driven by a MARTA strategic plan. She expressed concern that the process was occurring in absence of the city's comprehensive plan. She also pointed that the multiple claims of a partnership with the city had been made at the Charrette meeting. A discussion occurred about the authority of MARTA as a quasi-governmental organization. Council member Eyre commented on the responsibility of the city to be prepared for the regulatory aspects of impending rezoning request from MARTA.

Ms. Garrett referenced a letter that she received from MARTA which outlined their position of authority over their property. She offered further comments on the misrepresentation of the city's involvement in the TOD Charrette process. She reported that she had requested a meeting with MARTA and expressed a desire to have a discussion about City/MARTA relations with the General Manager. She also pointed out that many other property owners in the city had approached her with plans similar to MARTA's intentions and the council needed to determine if the city needed a large amount of multi-family developments. She expressed a desire to involve MARTA in the Buford Highway improvement plan process. Council member Mattison pointed out that the Jager Group had included MARTA connectivity in their proposal for the Buford Highway improvement plan. Council member Williams commented on the tools that the Development Authority possessed to encourage development in Brookhaven. She mentioned that South Face has asked to present the findings of the Charrette to the council members on Thursday morning. A discussion

occurred about scheduling conflicts with the time of the proposed meeting. Council member Eyre commented on the importance of the meeting. Council member Gebbia informed the public that the second court hearing for the Century Center lawsuit would be held on Thursday morning. Further discussion occurred about the court case and the South Face meeting.

[http://brookhavencityga.igam2.com/Citizens/Detail\\_Motion.aspx?Frame=&MeetingID=1086&MinutesID=1110&MediaPosition=5832.825&ID=3093&Type=50&CssClass=%20style=](http://brookhavencityga.igam2.com/Citizens/Detail_Motion.aspx?Frame=&MeetingID=1086&MinutesID=1110&MediaPosition=5832.825&ID=3093&Type=50&CssClass=%20style=)

Posted by [Melissa Weirman](#) on October 24, 2013.

## MARTA charrette yields ideas for improving Peachtree Road

A four-day charrette for the redevelopment of the Brookhaven-Oglethorpe MARTA station came to a close Oct. 24.

The charrette was a planning and brainstorming session for architects, urban planners, transportation experts, and stakeholders to discuss ways to make “transit-oriented development” at the MARTA station a reality.

Ideas from the charrette include improvements to Peachtree Road, such as a raised median and bike lanes, and improvements to Fernwood Park.

City officials also discussed potential zoning for the MARTA property, which is envisioned as a mixed-use development surrounding the station.

“We all see this to a certain extent as a city center,” said Councilman Jim Eyre.

<http://www.reporternewspapers.net/2013/10/24/marta-charrette-yields-ideas-improving-peachtree-road/>

# Brookhaven Oglethorpe Marta Station Project

## Local

ACBR partnered with NAR to provide \$15,000 to a Transit Oriented Development Charrette with Southface, a local advocate for sustainable communities which took place in October. This development will bring commercial and residential projects to the area immediately surrounding the Brookhaven-Oglethorpe MARTA station to increase the sense of a transit-oriented city center. Southface has completed a draft report for the Charrette and is collecting comments from the volunteer design team, the Atlanta Regional Commission, and MARTA. Once design team comments have been collected, at that point the report will be turned over to the ARC for final formatting and release to project partners and then the public.

The final Charrette report will be included as an attachment in the forthcoming MARTA RFP for redevelopment of the station's parking lots. MARTA is currently working with the City of Brookhaven on a timeline for the release of the RFP, it is anticipated that the RFP will be released in March. This RFP represents a great opportunity for the local real estate industry to develop a Transit Oriented Development at a prime location.

Monday, February 10, 2014

<http://www.atlcbtr.com/Resource-Center/news-page.aspx?ItemPath=%2FGlobal%2FNews+Content%2FGovernment%2FBrookhaven-Marta>



