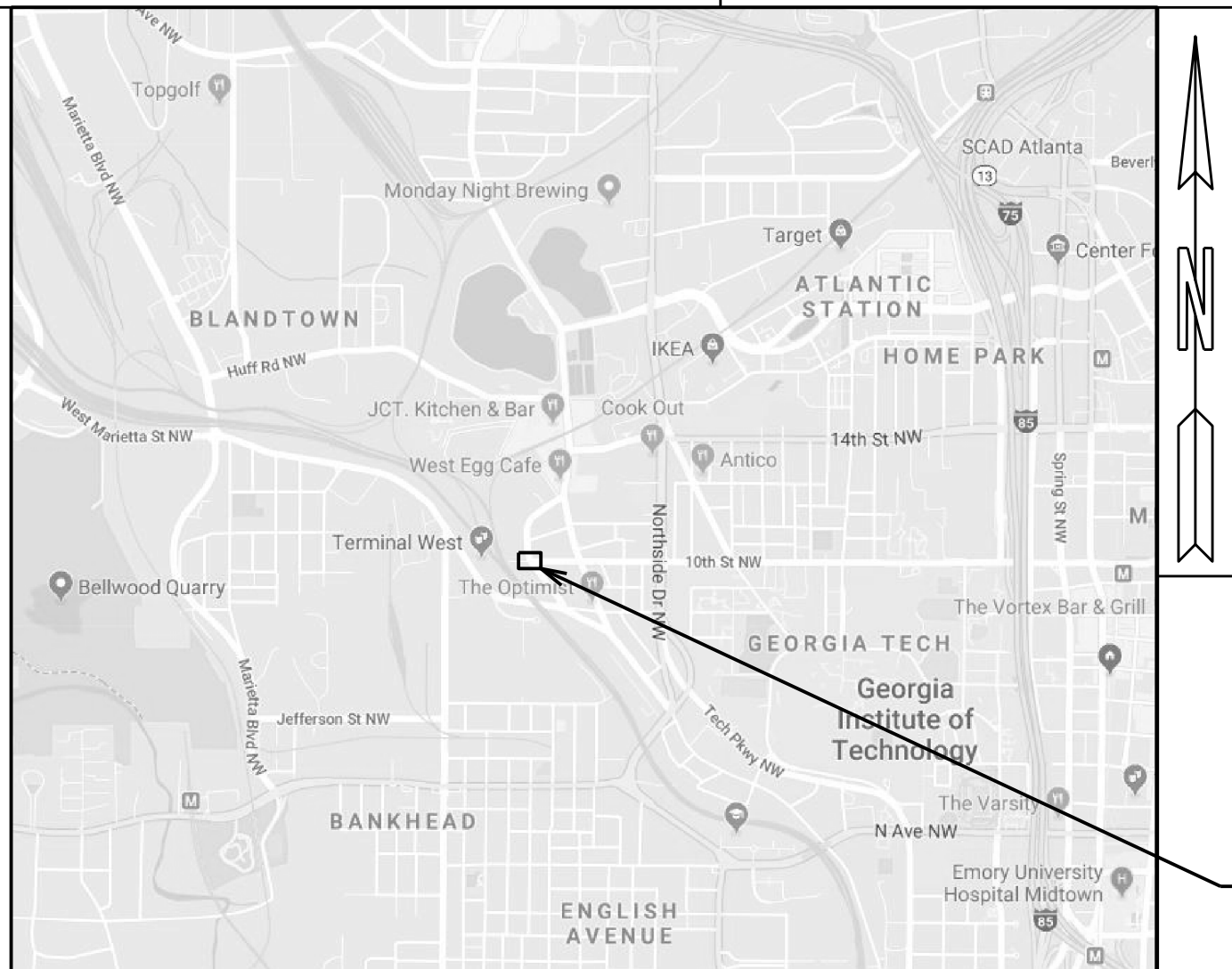


# UPPER WESTSIDE COMMUNITY IMPROVEMENT DISTRICT

## PLAN AND PROFILE OF PROPOSED 10TH STREET AT BRADY AVENUE ALL-WAY STOP DESIGN



LOCATION SKETCH

**SPEED DESIGN:**  
25 MPH BRADY AVENUE  
25 MPH 10TH STREET

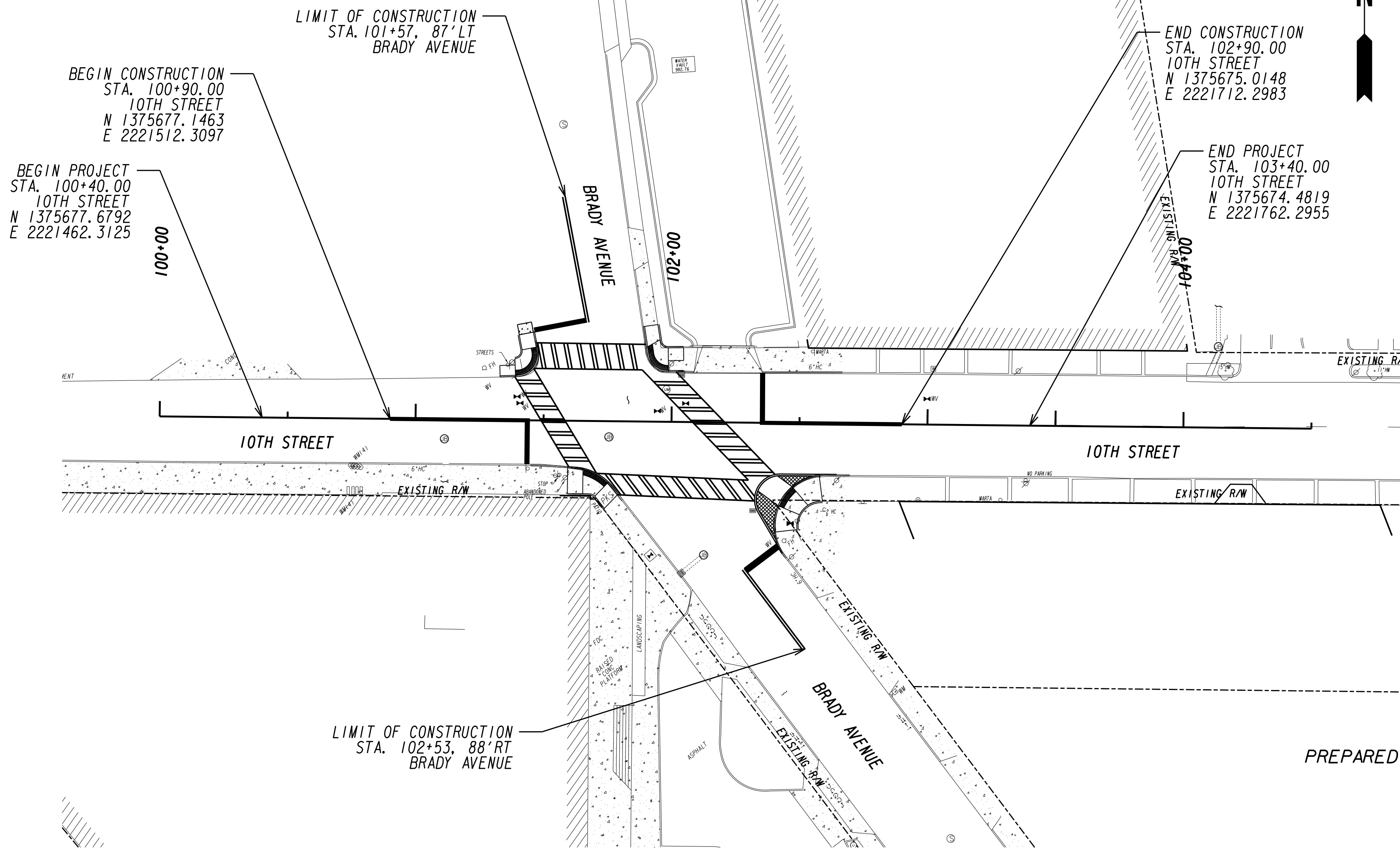
**FUNCTIONAL CLASS:**  
BRADY AVENUE - URBAN LOCAL ROAD  
10TH STREET - URBAN LOCAL ROAD

THIS PROJECT IS 100% IN  
FULTON COUNTY AND IS  
100% IN CONG. DIST. NO. 5.

THIS PROJECT HAS BEEN PREPARED  
USING THE HORIZONTAL GEORGIA  
COORDINATE SYSTEM OF 1984 (HAD  
1983/94 WEST ZONE, AND THE NORTH  
AMERICAN VERTICAL DATUM (NAVD)  
OF 1988.

THE DATA TOGETHER WITH ALL OTHER INFORMATION SHOWN ON THESE PLANS OR IN ANYWAY  
INDICATED THEREBY, WHETHER BY DRAWINGS OR NOTES, OR IN ANY OTHER MANNER, ARE BASED UPON  
FIELD INVESTIGATIONS AND ARE BELIEVED TO BE INDICATIVE OF ACTUAL CONDITIONS. HOWEVER, THE  
SAME ARE SHOWN AS INFORMATION ONLY, ARE NOT GUARANTEED, AND DO NOT BIND THE DEPARTMENT  
OF TRANSPORTATION IN ANY WAY. THE ATTENTION OF BIDDER IS SPECIFICALLY DIRECTED TO  
SUBSECTIONS 102.04, 102.05, AND 104.03 OF THE SPECIFICATIONS.

PROJECT LOCATION



**NOTE :**  
ALL REFERENCES IN THIS DOCUMENT, WHICH INCLUDES ALL PAPERS, WRITINGS,  
DOCUMENTS, DRAWINGS, OR PHOTOGRAPHS USED, OR TO BE USED IN CONNECTION  
WITH THIS DOCUMENT, TO "STATE HIGHWAY DEPARTMENT OF GEORGIA," "STATE  
HIGHWAY DEPARTMENT," "GEORGIA STATE HIGHWAY DEPARTMENT," "HIGHWAY  
DEPARTMENT," OR "DEPARTMENT" WHEN THE CONTEXT THEREOF MEANS THE  
STATE HIGHWAY DEPARTMENT OF GEORGIA, AND SHALL BE DEEMED TO MEAN  
THE DEPARTMENT OF TRANSPORTATION.

INDEX

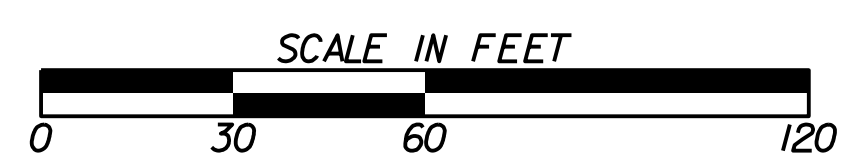
| DRAWING NO.          | DESCRIPTION   |
|----------------------|---|
| 01-0001              | COVER SHEET   |
| 04-0001              | GENERAL NOTES   |
| 26-0001              | SITE LAYOUT AND SIGNING AND MARKING PLANS             |
| 38-0001              | SPECIAL CONSTRUCTION DETAILS                          |
| CONSTRUCTION DETAILS |   |
| A3                   | CONCRETE SIDEWALK DETAILS CURB CUT (WHEELCHAIR) RAMPS |



**Kimley»Horn**

Engineering, Planning, and Environmental Consultants  
Suite 601, 817 West Peachtree Street, NW  
Atlanta, GA 30308

PREPARED BY: \_\_\_\_\_  
DESIGN



| PLANS COMPLETED | 05-10-2019 |
|-----------------|------------|
| REVISIONS       |            |
|                 |            |
|                 |            |
|                 |            |
|                 |            |
|                 |            |
|                 |            |
|                 |            |

DRAWING No.  
**01-0001**

**PROJECT NOTES**

1. A NOTICE OF INTENT IS NOT REQUIRED FOR THIS PROJECT.
2. UNLESS NOTED OTHERWISE, ALL EXISTING ITEMS WITHIN THE LIMITS OF WORK WILL BE RETAINED IN PLACE AND PROTECTED FROM DAMAGE DURING THE CONSTRUCTION PERIOD. ANY FACILITIES THAT ARE DAMAGED WILL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
3. SAFE, CLEARLY MARKED PEDESTRIAN AND VEHICULAR ACCESS TO ALL ADJACENT PROPERTIES MUST BE MAINTAINED THROUGHOUT THE DEMOLITION AND CONSTRUCTION PROCESS. SEE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.
4. THE GENERAL CONTRACTOR SHALL CONTACT THE PROPER LOCAL AUTHORITIES OR RESPECTIVE UTILITY COMPANY TO CONFIRM THE LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. ANY DAMAGE DUE TO FAILURE OF THE GENERAL CONTRACTOR TO CONTACT THE PROPER AUTHORITIES SHALL BE THE BORNE BY THE GENERAL CONTRACTOR.
5. UTILITY STRUCTURE/COVER WITHIN DEMOLITION AREA TO BE PROTECTED IN PLACE AND KEPT OPERATIONAL: CONTRACTOR TO VERIFY AND COORDINATE WITH ALL UTILITY COMPANIES.
6. THE GENERAL CONTRACTOR SHALL MAINTAIN OR ADJUST TO NEW FINISHED GRADE AS NECESSARY ALL UTILITY AND SITE STRUCTURES SUCH AS LIGHT POLES, SIGN POLES, MAN HOLES, CATCH BASINS, HAND HOLES, WATER AND GAS GATES, HYDRANTS, METER VALVES AND MISC. BOXES ETC. UNLESS OTHERWISE NOTED ON THE PLANS OR AS DIRECTED BY THE OWNER'S REPRESENTATIVE. IN THE EVENT A UTILITY OWNER IS TO RELOCATE ITS OWN FACILITIES, THE CONTRACTOR SHALL COORDINATE HIS WORK WITH THESE COMPANIES AND SHALL NOTIFY THEM OF WORK AFFECTING THEIR FACILITIES IN ADVANCE.
7. ALL REFUSE, DEBRIS AND MISCELLANEOUS ITEMS TO BE REMOVED, THAT ARE NOT BE STOCKPILED FOR LATER USE ON THE PROJECT OR DELIVERED TO THE OWNER, SHALL BE LEGALLY DISPOSED OF OFF-SITE BY THE GENERAL CONTRACTOR.
8. ALL ITEMS AND STRUCTURES REQUIRING REMOVAL SHALL BE REMOVED TO FULL DEPTH TO INCLUDE BASE MATERIAL AND FOOTINGS OR FOUNDATIONS AS APPLICABLE.
9. WHERE NEW CONCRETE OR ASPHALT PAVEMENT, SIDEWALK, CURB RAMPS, DRIVE APRONS, CURB, ETC. MEET EXISTING PAVEMENT, EXISTING SHALL BE SAWCUT TO PROVIDE A SMOOTH, CLEAN EDGE.
10. WHERE NEW PAVING MEETS EXISTING PAVING, MEET LINE AND GRADE OF EXISTING WITH NEW CONSTRUCTION.
11. DAMAGE NO PLANTS TO REMAIN BY BURNING, BY PUMPING OF WATER, BY CUTTING OF LIVE ROOTS OR BRANCHES, OR BY ANY OTHER MEANS. NO PLANTS TO BE SAVED SHALL BE USED FOR CRANE STAYS, GUYS OR OTHER FASTENINGS. VEHICLES SHALL NOT BE PARKED OR LEFT TEMPORARILY STANDING IDLE WITHIN THE DRIP LINE OF WHERE DAMAGE MAY RESULT TO TREES TO BE SAVED. CONSTRUCTION MATERIALS SHALL NOT BE STORED BENEATH TREES TO BE SAVED.
12. CONTRACTOR SHALL HYDROBLAST ALL EXISTING CONFLICTING STRIPING WITHIN PROJECT LIMITS.
13. ANY SIDEWALK, PAVERS, HARDSCAPE OR LANDSCAPE DISTURBED BY THE CONTRACTOR SHALL BE RETURNED TO ITS ORIGINAL CONDITION. THE COST FOR THIS SHALL BE INCLUDED IN THE OVERALL BID PRICE AND WILL NOT BE PAID FOR SEPARATELY.
14. ANY CURB THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED IN KIND BY THE CONTRACTOR AT NO COST TO THE PROJECT.
15. FINISH GRADES: THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL NEW SIDEWALK AREAS ARE GRADED TO DRAIN, EITHER TO EXISTING OR NEW STRUCTURES. CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL ACCESSIBILITY CODES.
16. THE CONTRACTOR SHALL REMOVE ALL SILT & DEBRIS FROM EXISTING DRAINAGE STRUCTURES, PIPES & CULVERTS THAT ARE RETAINED BEFORE ANY WORK BEGINS & AT THE COMPLETION OF THE PROJECT.
17. ALL SIGNING, MARKING AND TRAFFIC CONTROL SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.
18. THE CONTRACTOR WILL BE RESPONSIBLE FOR PREPARING A TRAFFIC CONTROL PLAN SHOWING THE PROPOSED MEASURES TO MANAGE TRAFFIC DURING CONSTRUCTION ACTIVITIES. THE PLAN SHALL CONFORM TO THE CURRENT MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. ANY LANE CLOSURES MUST BE APPROVED BY AND COORDINATED WITH THE CITY OF ATLANTA. LANE CLOSURES WILL REQUIRE PROPER LANE TAPERS AND ADVANCE WARNINGS PER THE MUTCD, CURRENT EDITION.
19. THERE SHALL BE NO INCREASE IN FLOWS, OR COMPENSATION IN OTHER DRAINAGE AREAS, WHICH RESULT IN AN INCREASED PEAK DISCHARGE ONTO ADJACENT PROPERTY. EACH DRAINAGE AREA LEAVING SITE SHALL BE STUDIED AND CONTROLLED.

20. ALL REQUIRED IMPROVEMENTS SHALL COMPLY WITH PUBLIC LAW 101-336, THE AMERICANS WITH DISABILITIES ACT OF 1990, AS AMENDED OR MOST CURRENT. ANY IMPROVEMENTS WHICH DO NOT COMPLY WITH SAID ACT SHALL BE REDONE AT THE CONTRACTOR'S EXPENSE AND FINAL APPROVAL OF THE CONSTRUCTION SHALL NOT BE GIVEN UNTIL SUCH WORK IS COMPLETED IN COMPLIANCE WITH THE ACT.
21. ALL ADA RAMPS AND SIDEWALK WITHIN THE INTERSECTION RADII ARE TO BE 8 INCH CONCRETE. THE COST FOR ADA RAMPS INCLUDING DETECTABLE WARNING STRIPS SHALL BE INCLUDED IN THE PRICE BID FOR 8 INCH CONCRETE SIDEWALK.
22. PLANS SHALL CONFORM TO ALL CITY OF ATLANTA AND GEORGIA DEPARTMENT OF TRANSPORTATION STANDARDS AND SPECIFICATIONS.
23. ALL EXISTING SIGNS DISTURBED DURING CONSTRUCTION SHALL BE RESET IN THEIR EXISTING LOCATION UNLESS OTHERWISE NOTED.
24. ALL STRIPING SHALL BE THERMOPLASTIC TRAFFIC STRIPING PER CITY OF ATLANTA AND GEORGIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.

**GENERAL NOTES - STANDARD SIGNS**

1. ALL STANDARD HIGHWAY SIGNS SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH THE DETAILS SHOWN IN THE PLANS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, AND CITY OF ATLANTA STANDARDS, AND THE GEORGIA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND/OR SPECIAL PROVISIONS.
2. SIGN ERECTION STATIONS ARE APPROXIMATE AND MAY BE ADJUSTED TO MEET FIELD CONDITIONS WHERE NECESSARY, BUT SHALL BE WITHIN THE LIMITATIONS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION. NO SIGN LOCATION SHALL BE CHANGED BY THE CONTRACTOR OR BY THE PROJECT ENGINEER WITHOUT PRIOR APPROVAL FROM THE OWNER'S REPRESENTATIVE AND THE CITY OF ATLANTA.
3. ALL STANDARD HIGHWAY SIGNS SHALL BE ERECTED AT A HEIGHT OF 7 FEET ABOVE THE NORMAL EDGE OF PAVEMENT TO THE BOTTOM OF THE SIGN OR ASSEMBLY.
4. SINGLE PLATE, HORIZONTAL RECTANGULAR SIGNS OVER 48 INCHES IN WIDTH SHALL BE MOUNTED ON TWO POSTS WITH 2 EACH 2 INCH x 1/2 INCH x (WIDTH OF SIGN) ALUMINUM OR GALVANIZED STEEL STRAPS. THE STRAPS SHALL BE FLUSH WITH THE BACK OF THE SIGN WITH ONE EACH ACROSS THE TOP AND BOTTOM OF THE SIGN. THE CENTERLINE OF EACH POST SHALL BE INSET 1/6TH OF THE SIGN WIDTH FROM THE EDGE OF THE SIGN. SIGN PLATE BOLT HOLES SHALL BE 1/2 INCH DIAMETER, DRILLED OR PUNCHED, AS SHOWN ON THE SIGN PLATE DETAILS.
5. EACH 42 OR 48 INCH WIDE x 18 OR 24 INCH HIGH SIGN REQUIRES ONE 2 INCH x 1/2 INCH x (WIDTH OF SIGN) ALUMINUM OR GALVANIZED STEEL STRAP LOCATED IN THE CENTER OF THE SIGN AND FLUSH WITH THE BACK OF THE SIGN.
6. SIGN ASSEMBLIES SHALL BE MOUNTED ON ALUMINUM OR GALVANIZED STEEL STRAP FRAMES. FOR DETAILS AND STRAP SPECIFICATIONS REFER TO SIGN ASSEMBLY-TYPICAL FRAMING DETAILS.
7. TYPE 9 (HIGH INTENSITY) REFLECTIVE SHEETING SHALL BE USED FOR ALL STANDARD HIGHWAY SIGNS REQUIRING REFLECTORIZED BACKGROUNDS EXCEPT AS SPECIFIED BELOW OR SPECIFIED OTHERWISE IN THE PLANS. EITHER CLASS 1 OR CLASS 2 ADHESIVE BACKING IS PERMISSIBLE.
8. TYPE 11 (VERY HIGH INTENSITY) REFLECTIVE SHEETING SHALL BE USED FOR ALL RED SERIES SIGNS (R1-1, R1-2, R1-3P, R5-1, R5-1A, R5-1B).
9. TYPE 11 (VERY HIGH INTENSITY) FLUORESCENT YELLOW REFLECTIVE SHEETING SHALL BE USED FOR ALL WARNING SIGNS.
10. TYPE 11 (VERY HIGH INTENSITY) FLUORESCENT YELLOW GREEN REFLECTIVE SHEETING SHALL BE USED FOR SCHOOL ZONE (S1-1, S2-1, S3-1, S4-3, AND THE TOP PORTION OF THE S5-1) SIGN. ALL REGULATORY SIGNS WITHIN THE SCHOOL ZONE SHALL HAVE TYPE 11 (VERY HIGH INTENSITY) REFLECTIVE SHEETING.
11. A 1/2 INCH MINIMUM AIR SPACE SHALL BE REQUIRED BETWEEN ALL SIGN PLATES WITHIN AN ASSEMBLY.
12. WHERE SIGNS WITHIN AN ASSEMBLY EXTEND BELOW THE STANDARD MOUNTING HOLES ON THE POST(S), ADDITIONAL 1/2 INCH DIAMETER HOLE(S), DRILLED OR PUNCHED, SHALL BE REQUIRED TO PROPERLY MOUNT THE ASSEMBLY.
13. INTERSTATE SHIELDS SHALL CONTAIN THE WORD GEORGIA. ALL INTERSTATE, U.S., AND GEORGIA SHIELDS REQUIRING ALT, BUS, CONN, LOOP, OR SPUR SHALL USE 4 INCH SERIES "D" LETTERS. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, FOR DETAILS.
14. FOR DETAILS OF SPECIAL DESIGN HIGHWAY SIGNS, SEE DETAILS OF MISCELLANEOUS SIGNS.
15. REMOVE ANY EXISTING SIGNAGE WITHIN THE PROJECT LIMITS AS FOLLOWS: ONE-WAY SIGNAGE WHERE OPERATION IS BEING CONVERTED TO TWO-WAY, ANY SIGNAGE IN CONFLICT WITH OR NOT CONSISTENT WITH THE PROPOSED PROJECT, AND ANY SIGNS TO BE REMOVED AT THE DIRECTION OF THE ENGINEER. COST TO REMOVE SIGNS SHALL BE INCLUDED IN THE OVERALL BID PRICE.
16. THE CONTRACTOR WILL, AS REQUESTED BY THE ENGINEER, BE REQUIRED TO REMOVE ANY EXISTING SIGNS THAT ARE DUPLICATED OR ARE CONTRARY TO THESE SIGN PLANS.



**REVISION DATES**

|  |  |  |
|--|--|--|
|  |  |  |
|  |  |  |
|  |  |  |
|  |  |  |
|  |  |  |
|  |  |  |
|  |  |  |
|  |  |  |

**GENERAL NOTES**

10TH STREET AT BRADY AVENUE  
 ALL-WAY STOP

|              |       |                               |
|--------------|-------|-------------------------------|
| CHECKED:     | DATE: | DRAWING No.<br><b>04-0001</b> |
| BACKCHECKED: | DATE: |                               |
| CORRECTED:   | DATE: |                               |
| VERIFIED:    | DATE: |                               |

**NORTHWEST RAMP DETAIL:**  
 THIS DETAIL IS FOR  
 SUPPLEMENTAL INFORMATION ONLY.  
 RAMP SHALL MEET ADA REQUIREMENTS.



CONTRACTOR TO SAWCUT AND REMOVE DAMAGED PAVEMENT  
 AT CORNER; REPLACE WITH CLASS B CONCRETE  
 BEGIN PROJECT  
 STA. 100+40.00  
 10TH STREET  
 N 1375677.6792  
 E 2221462.3125

DO NOT DISTURB  
 EXISTING UTILITY  
 POLE  
 BEGIN CONSTRUCTION  
 TIE TO EXISTING STRIPING  
 STA. 100+90.00  
 10TH STREET  
 N 1375677.1463  
 E 2221512.3097

8" SOLID DOUBLE YELLOW  
 STA. 101+44.19' RT  
 30' OCT  
 R1-1  
 R1-3P  
 18' X 6'

DO NOT DISTURB EXISTING UTILITY POLES  
 INSTALL TP B CURB RAMP  
 50' RED PAINTED CURB

LIMIT OF CONSTRUCTION  
 TIE TO EXISTING STRIPING  
 STA. 102+53.88' RT  
 BRADY AVENUE

8" SOLID DOUBLE YELLOW  
 STA. 101+44.18' LT  
 30' OCT  
 R1-1  
 R1-3P  
 18' X 6'

4" CONCRETE SIDEWALK;  
 SAWCUT EXISTING SIDEWALK AT JOINT  
 AND REPLACE EXISTING PANEL  
 INSTALL TP B CURB RAMP;  
 SEE NORTHWEST RAMP DETAIL

LIMIT OF CONSTRUCTION  
 TIE TO EXISTING STRIPING  
 STA. 101+57.87' LT  
 BRADY AVENUE

INSTALL TP B CURB RAMP  
 DO NOT DISTURB EXISTING UTILITY POLE  
 DO NOT DISTURB EXISTING DRAINAGE STRUCTURE

STOP  
 ALL WAY  
 30' OCT  
 R1-1  
 R1-3P  
 18' X 6'  
 STA. 102+35.28' LT

24" SOLID WHITE (TYP.)  
 8" SOLID DOUBLE YELLOW

END CONSTRUCTION  
 TIE TO EXISTING STRIPING  
 STA. 102+90.00  
 10TH STREET  
 N 1375675.0148  
 E 2221712.2983

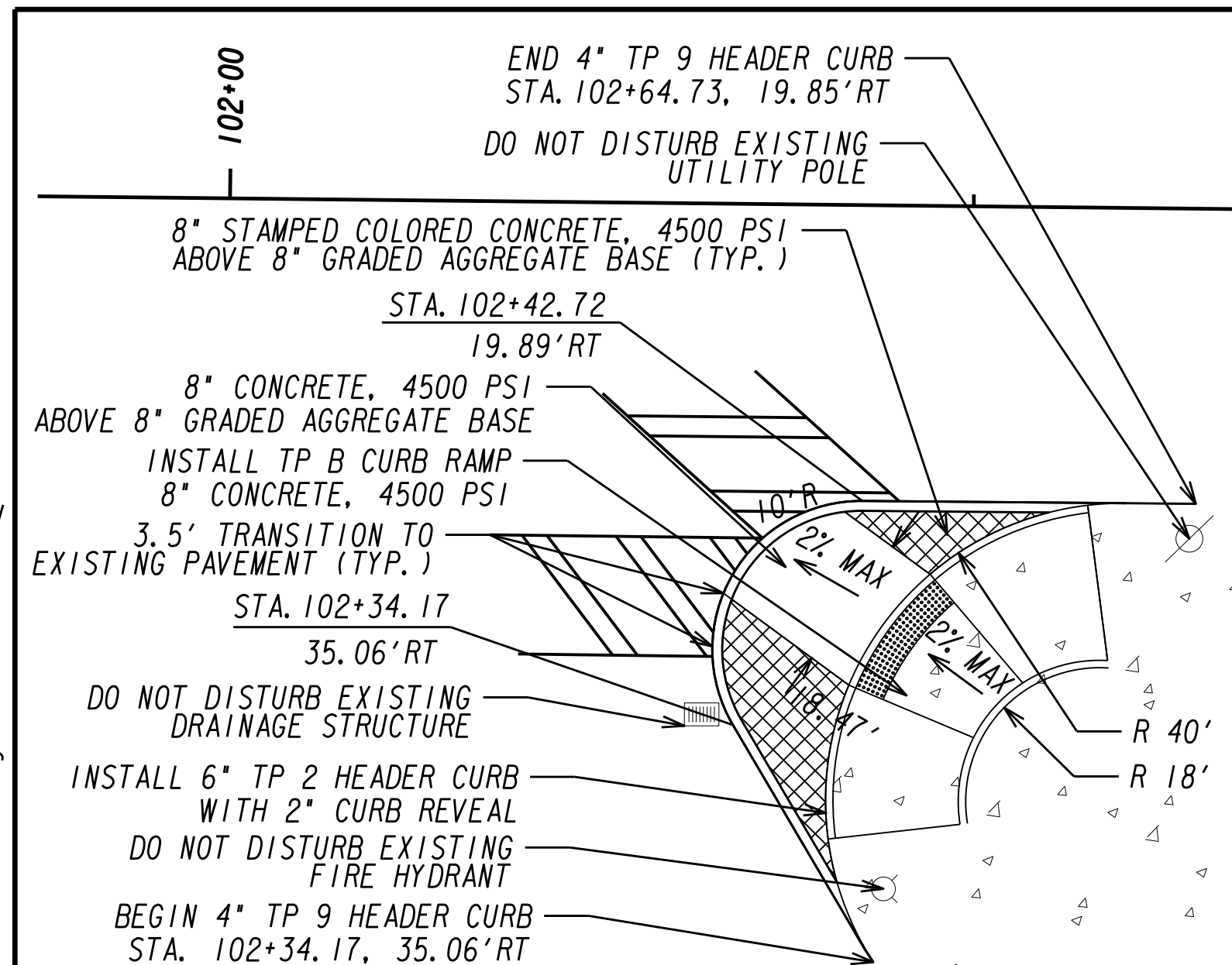
END PROJECT  
 STA. 103+40.00  
 10TH STREET  
 N 1375674.4819  
 E 2221762.2955

8" SOLID WHITE (T-11A) (TYP.)  
 SEE APRON DETAIL

STOP  
 ALL WAY  
 30' OCT  
 R1-1  
 R1-3P  
 18' X 6'  
 STA. 102+48.43' RT

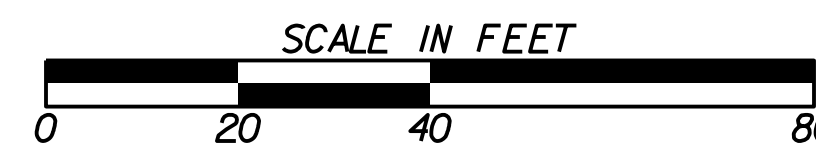
8" SOLID DOUBLE YELLOW  
 STA. 102+48.43' RT  
 30' OCT  
 R1-1  
 R1-3P  
 18' X 6'

**APRON DETAIL**



**Kimley»Horn**

Engineering, Planning, and Environmental Consultants  
 Suite 601, 817 West Peachtree Street, NW  
 Atlanta, GA 30308



**REVISION DATES**

| NO. | DATE | DESCRIPTION |
|-----|------|-------------|
|     |      |             |
|     |      |             |
|     |      |             |
|     |      |             |

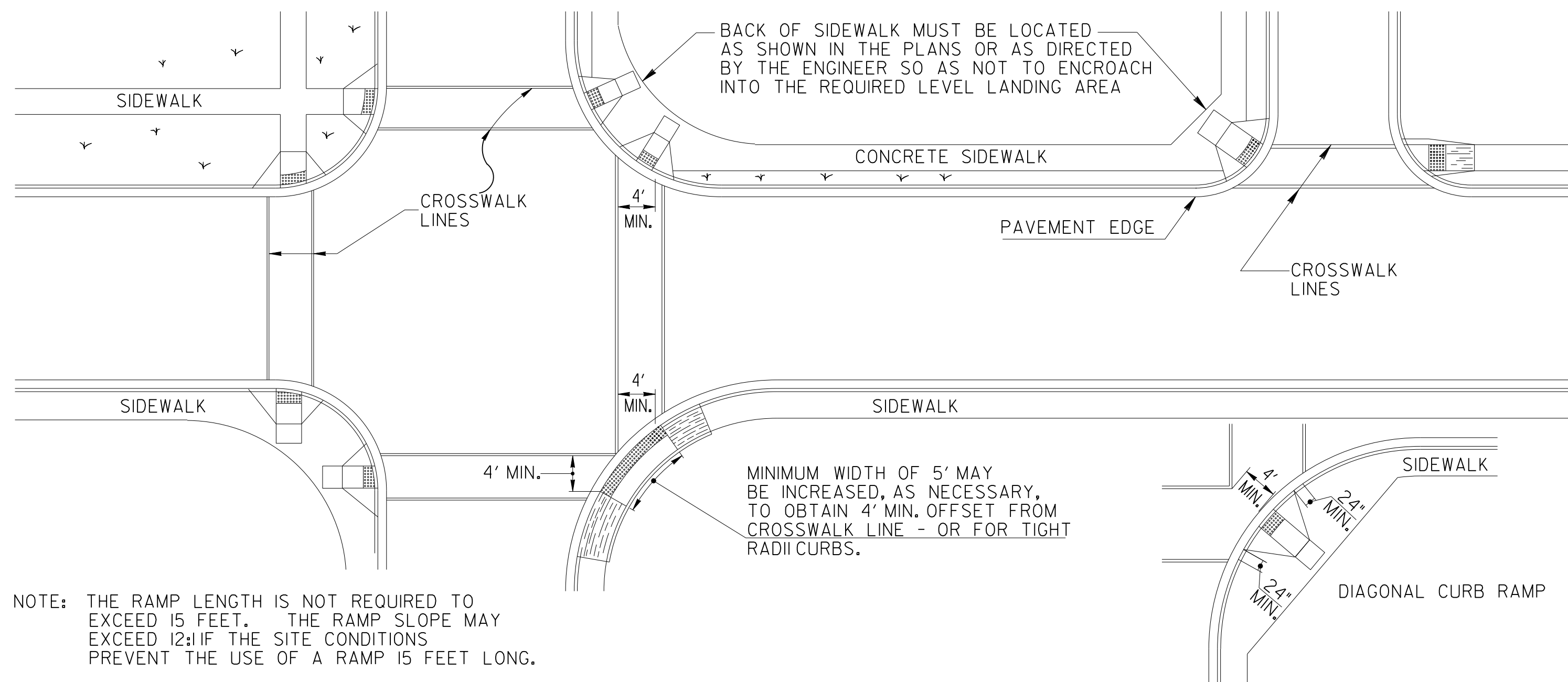
**SITE LAYOUT AND  
 SIGNING AND MARKING PLAN**

10TH STREET AT BRADY AVENUE  
 ALL-WAY STOP

|              |       |             |
|--------------|-------|-------------|
| CHECKED:     | DATE: | DRAWING No. |
| BACKCHECKED: | DATE: | 26-0001     |
| CORRECTED:   | DATE: |             |
| VERIFIED:    | DATE: |             |

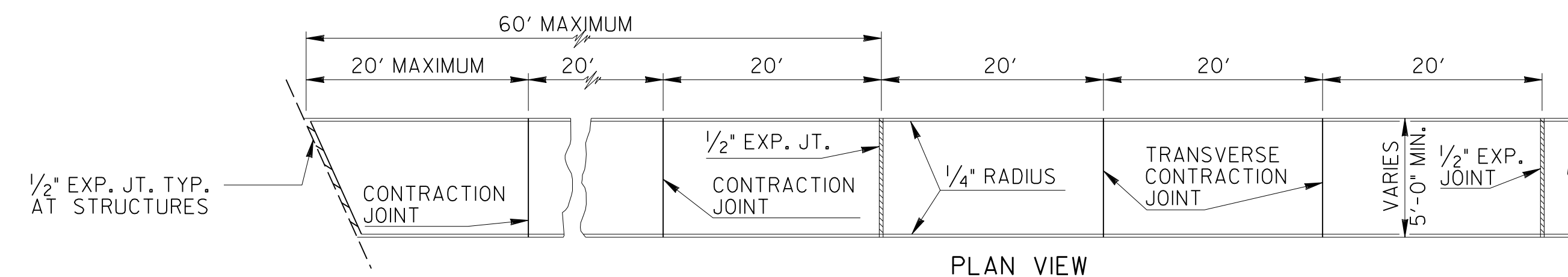


### TYPICAL LOCATIONS FOR CURB CUT RAMPS - PLAN VIEW

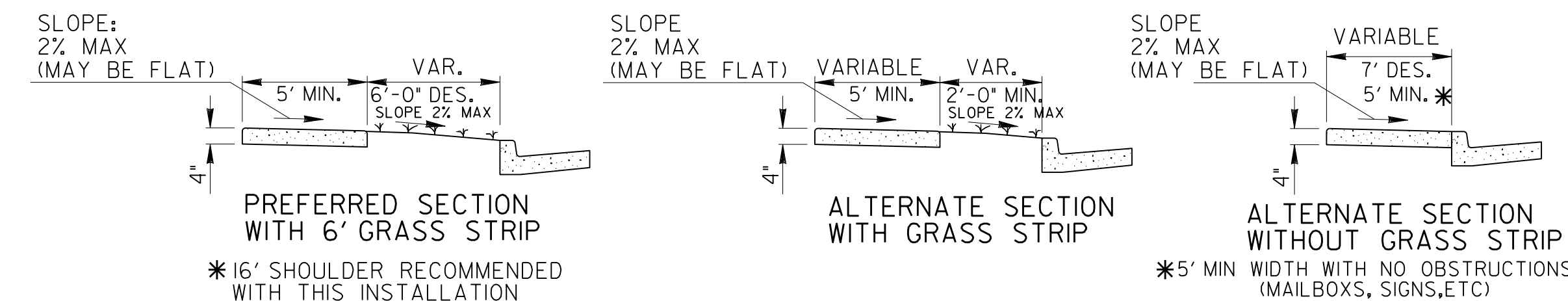
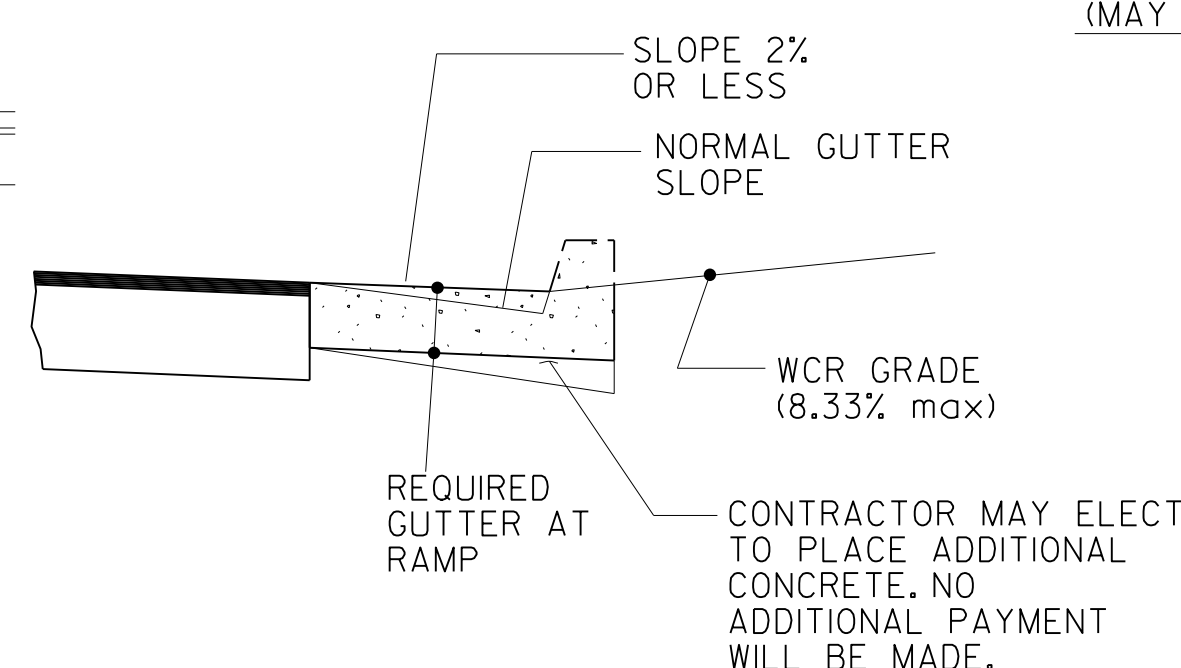


NOTE: THE RAMP LENGTH IS NOT REQUIRED TO EXCEED 15 FEET. THE RAMP SLOPE MAY EXCEED 12% IF THE SITE CONDITIONS PREVENT THE USE OF A RAMP 15 FEET LONG.

### CONCRETE SIDEWALK DETAILS



### GUTTER TRANSITION DETAIL



NOTES FOR CONCRETE SIDEWALK:

- CONCRETE TO BE PLACED 4" THICK AND FINISHED WITH TAMPS, WOOD FLOATS AND STIFF-BRISTLE BOOMS.
- TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED AT 20 FT. INTERVALS. ALL EDGES TO BE ROUNDED TO 1/4" RADIUS.
- 1/2" EXPANSION JOINTS SHALL BE PLACED, WHERE SIDEWALK TIE INTO A STRUCTURE OR TERMINATE AT CURB, RAMPS OR DRIVEWAYS AND AT 60' INTERVALS.

NOTES FOR CURB CUT RAMPS:

- CURB CUT RAMPS WILL BE LOCATED AS FOLLOWS UNLESS PLANS OR CONTRACT SPECIFY OTHERWISE.
  - AT ALL PEDESTRIAN CROSSWALKS WHERE CURB IS CONSTRUCTED OR REPLACED.
  - WHERE THE SIDEWALK, CONCRETE OR UNPAVED, IS INTERRUPTED BY THE CURB AT TURNOUTS OR AT INTERSECTIONS.
  - AT OTHER LOCATIONS SUCH AS HOSPITALS, NURSING HOMES, REST AREAS, ETC., WHERE THE CURB WOULD OTHERWISE BE AN OBSTRUCTION TO THE PHYSICALLY DISABLED.
- RAMPS WILL BE CONSTRUCTED FROM CONCRETE. SPECIFICATIONS FOR RAMPS WILL BE THE SAME AS FOR CONCRETE SIDEWALK. RAMPS SHALL HAVE EITHER A ROUGH OR A TEXTURED FINISH.
- DROP INLETS ARE NOT TO BE LOCATED DIRECTLY IN FRONT OF RAMPS. CATCH BASINS SHOULD BE LOCATED AT LEAST 10 FT. FROM RAMPS WHEN FEASIBLE.
- WHERE RAMPS ARE LOCATED IN RADII, THE DIMENSIONS SHOWN FOR RAMP WIDTHS AND TAPERS ARE MEASURED PERPENDICULAR TO THE RAMP AND NOT ALONG THE CURVE.
- WHERE UTILITY STRUCTURES CONFLICT, WHERE SIDEWALK GEOMETRY VARIES, AT SKEWED INTERSECTIONS, OR IN OTHER SPECIAL CASES, THE RAMP DESIGNS MAY BE MODIFIED BY THE DESIGNER OR ENGINEER, PROVIDED THAT THE WIDTH REMAINS A MINIMUM OF 48 INCHES, AND NO SLOPE ON THE ACCESSIBLE PART OF THE RAMP IS STEEPER THAN 12%.
- 1 IN. FT. OF CURB AND GUTTER WILL INCLUDE THE TRANSITIONED CURB IN FRONT OF RAMPS. SO. YDS. OF CONCRETE SIDEWALK AND CONCRETE MEDIAN PAVING WILL INCLUDE RAMPS. NO ADDITIONAL PAYMENT WILL BE MADE FOR CURB RAMPS. NO ADDITIONAL PAYMENT WILL BE MADE FOR SAWING AND REMOVING EXISTING SIDEWALK OR CURB WHERE NECESSARY FOR RAMP CONSTRUCTION.
- WHEN A CURB RAMP IS PLACED ON EXISTING PAVEMENT, THE PAVEMENT SHALL BE REMOVED TO PROVIDE A MINIMUM THICKNESS OF 3 INCHES OF CONCRETE AT ALL LOCATIONS. NO SEPARATE PAYMENT WILL BE MADE FOR REMOVAL OF THE PAVEMENT.
- DETECTABLE WARNING SURFACES ARE REQUIRED ON ALL INTERSECTIONS WITH PUBLIC STREETS, SIGNALIZED COMMERCIAL DRIVEWAYS, AND COMMERCIAL DRIVEWAYS WITH AN AADT OF 25 VPD.

### Type A

(Perpendicular)  
(The Preferred Ramp)

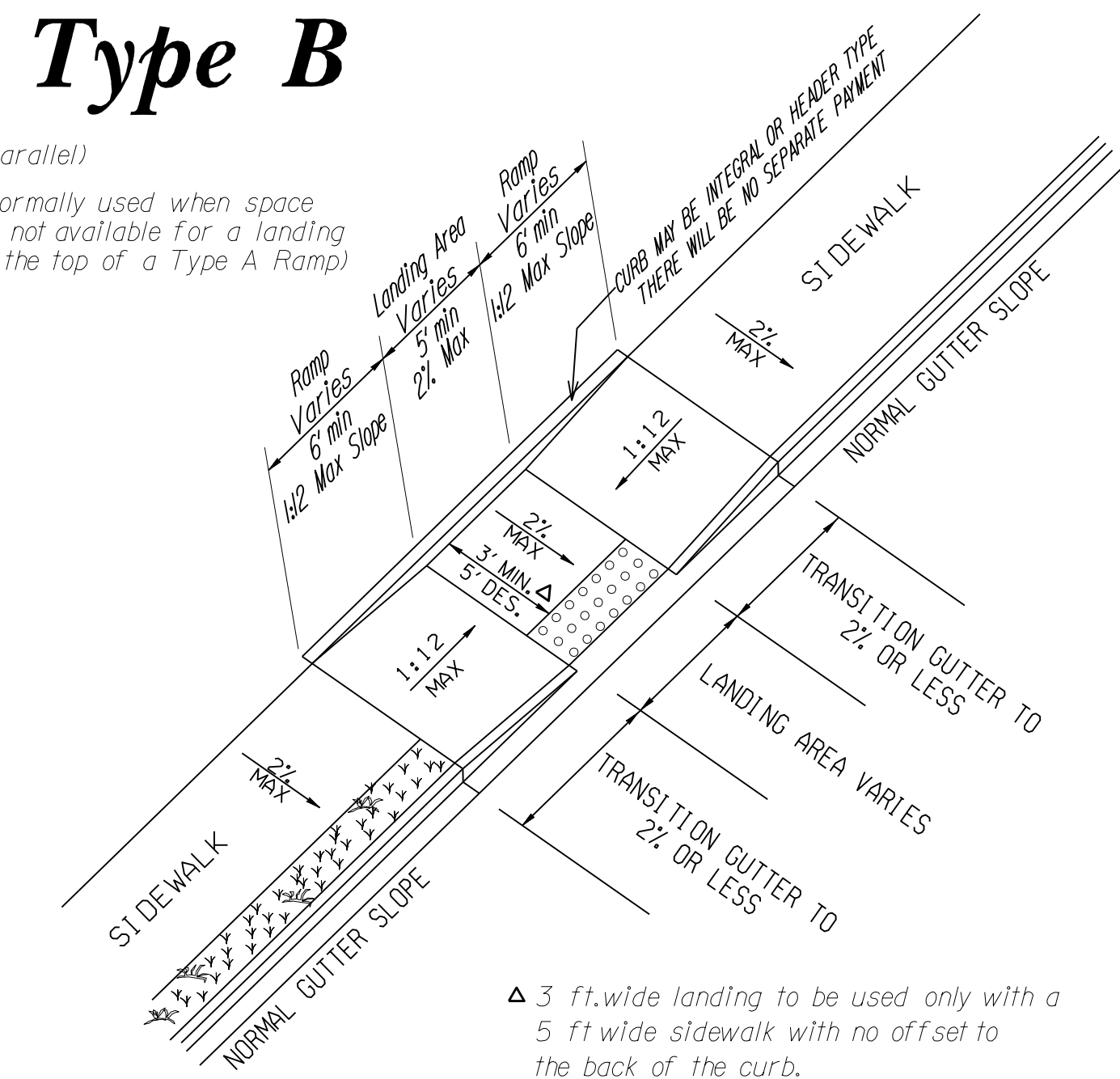
Back of sidewalk shall be located as shown in the plans or as directed by the Engineer so as not to encroach into the required landing area.

| LENGTH REQUIRED FOR A 1:10 SLOPE |                 |
|----------------------------------|-----------------|
| DIFFERENCE IN HEIGHT             | LENGTH REQUIRED |
| 1 inch                           | 10 inches       |
| 2 inches                         | 1'-8"           |
| 3 inches                         | 2'-6"           |
| 4 inches                         | 3'-4"           |
| 5 inches                         | 4'-2"           |
| 6 inches                         | 5 feet          |

### Type B

(Parallel)

(Normally used when space is not available for a landing at the top of a Type A Ramp)

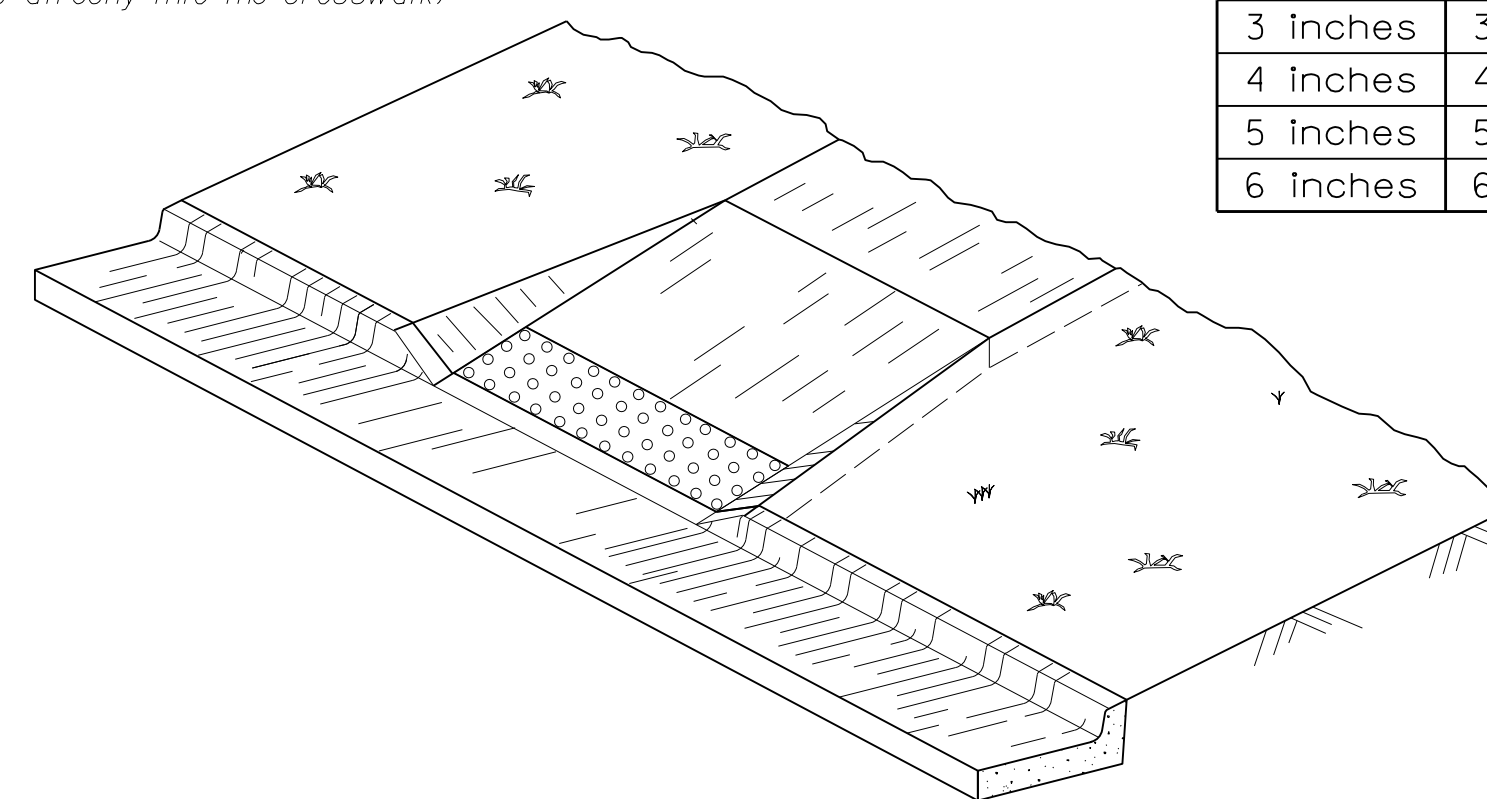


3 ft. wide landing to be used only with a 5 ft wide sidewalk with no offset to the back of the curb.

### Type D

(Perpendicular)

(Normally used when the sidewalk ties directly into the crosswalk)

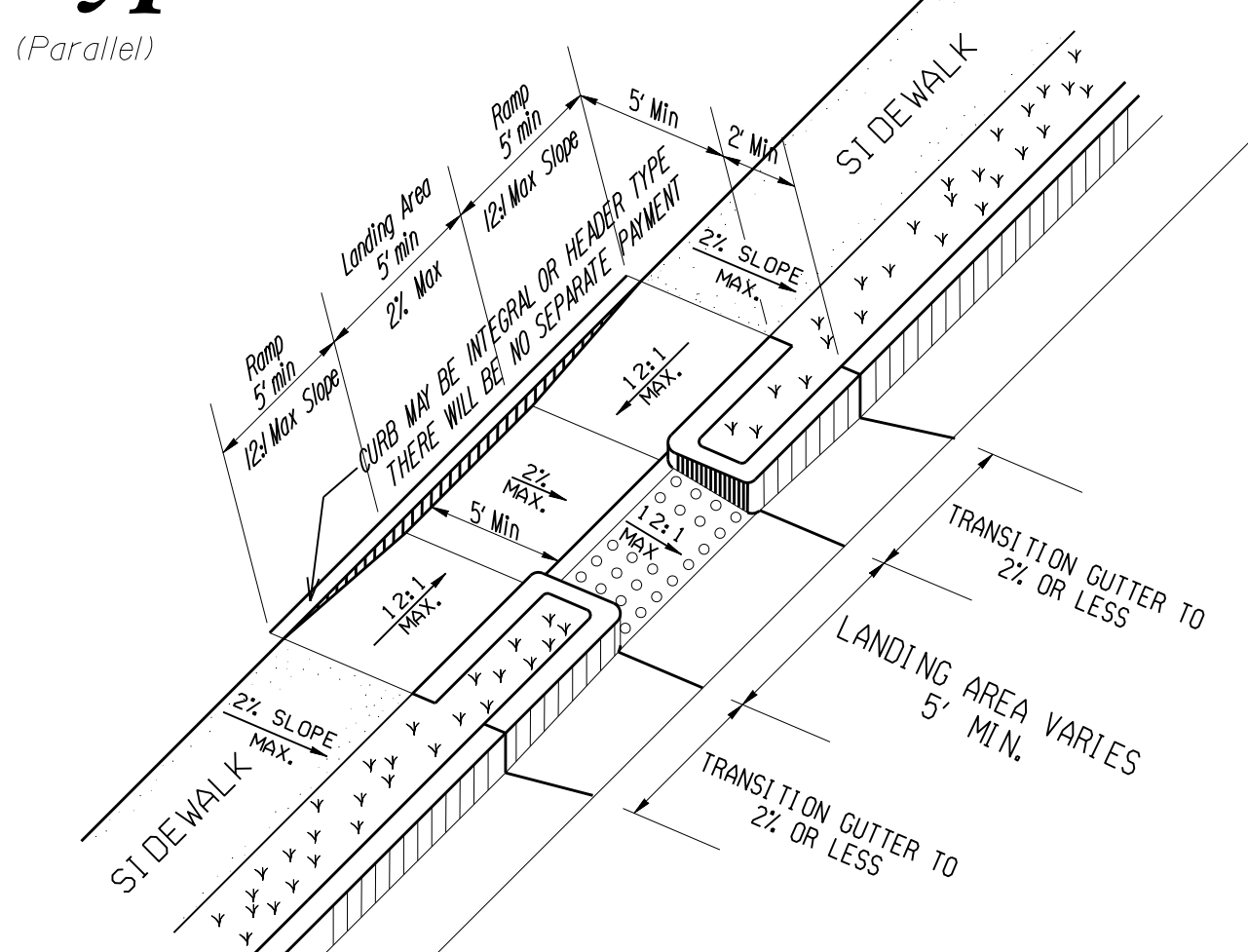


IN AREAS WHERE THE GUTTER HAS A SLOPE 1" IN 1' END NORMAL GUTTER SLOPE AT A DISTANCE OF 6 TO 10 FEET FROM THE RAMP AND BEGIN TRANSITION TO A FLAT GUTTER SLOPE. NORMAL GUTTER SLOPE SHALL BE RESUMED AT A SIMILAR DISTANCE BEYOND THE RAMP.

| LENGTH REQUIRED FOR A 1:12 SLOPE |                 |
|----------------------------------|-----------------|
| DIFFERENCE IN HEIGHT             | LENGTH REQUIRED |
| 1 inch                           | 1 foot          |
| 2 inches                         | 2 feet          |
| 3 inches                         | 3 feet          |
| 4 inches                         | 4 feet          |
| 5 inches                         | 5 feet          |
| 6 inches                         | 6 feet          |

### Type C

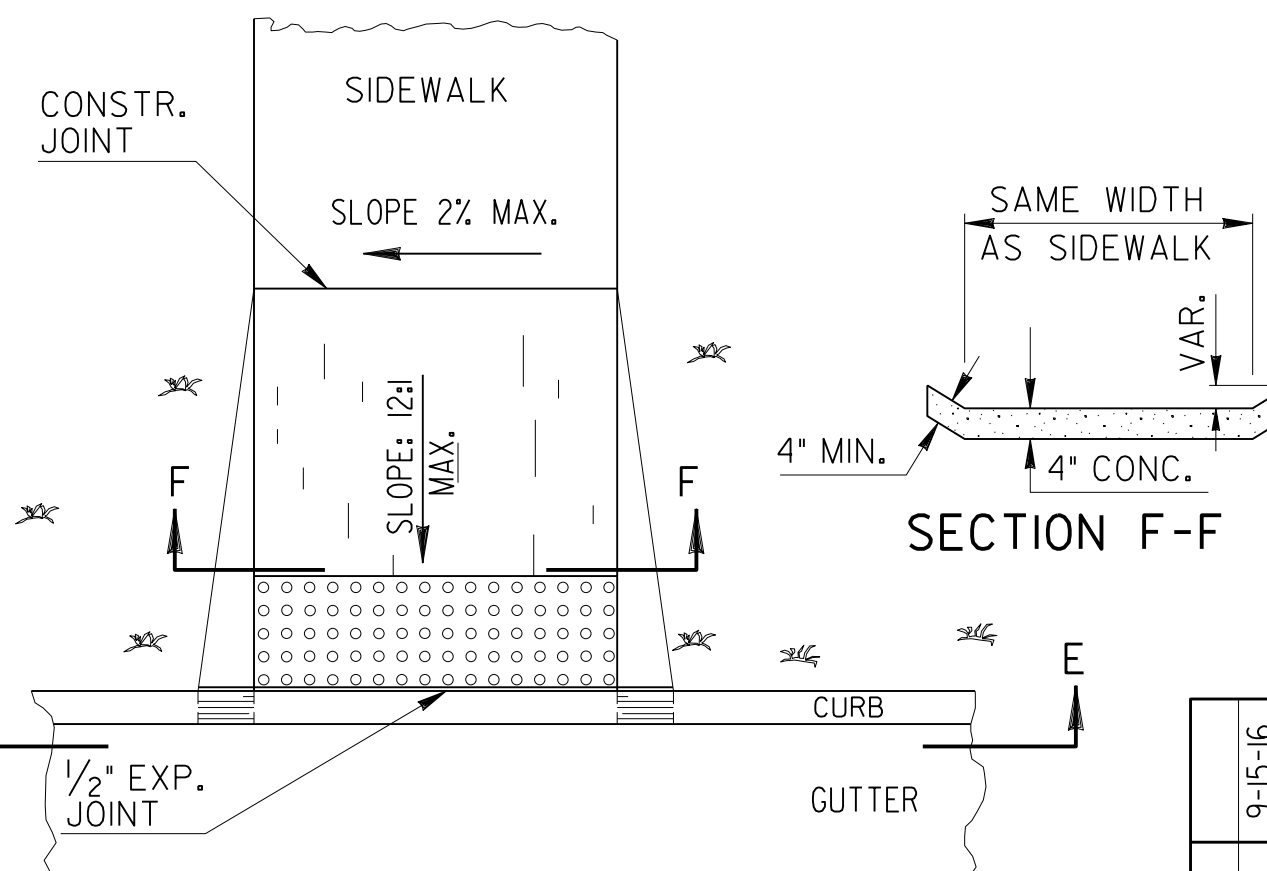
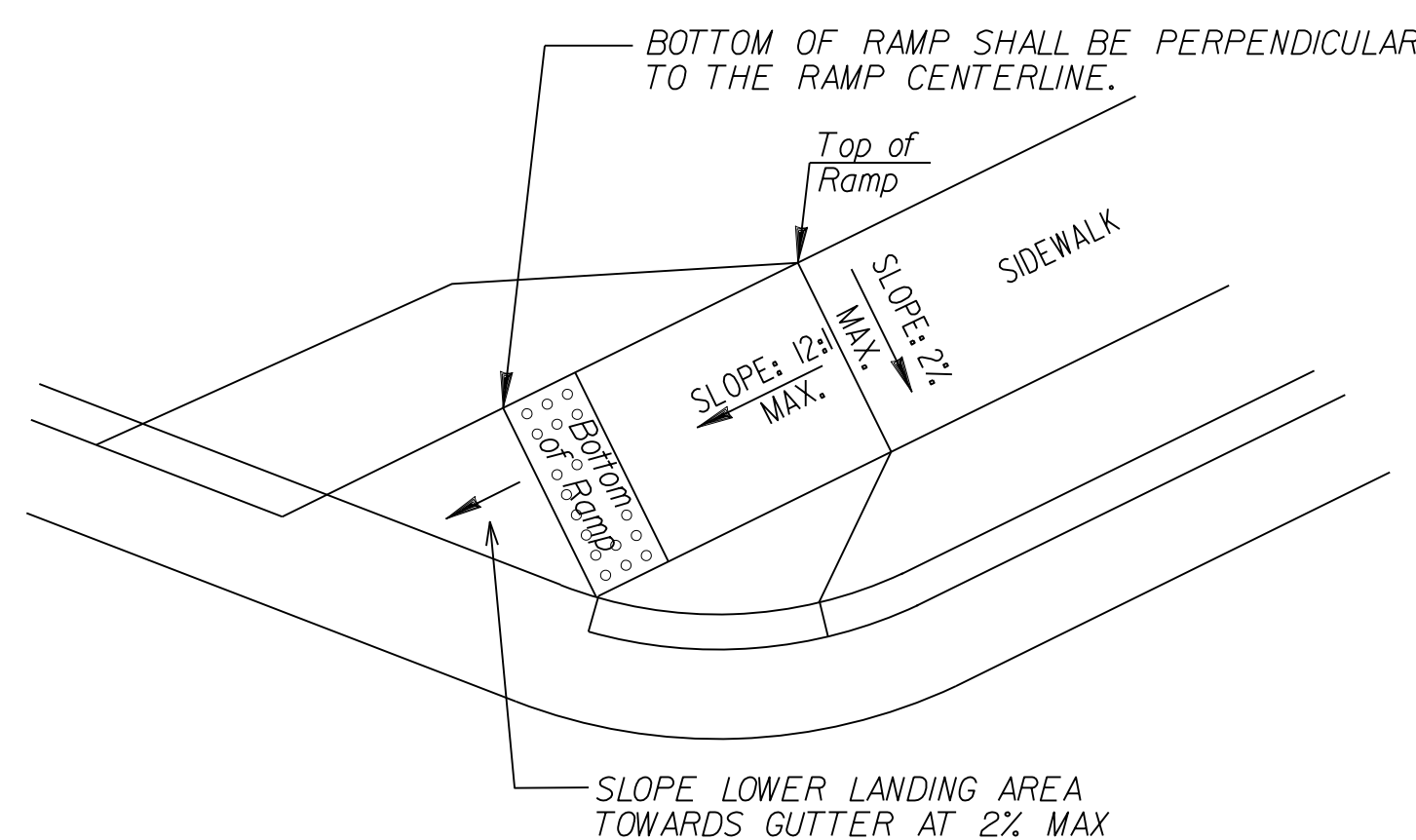
(Parallel)



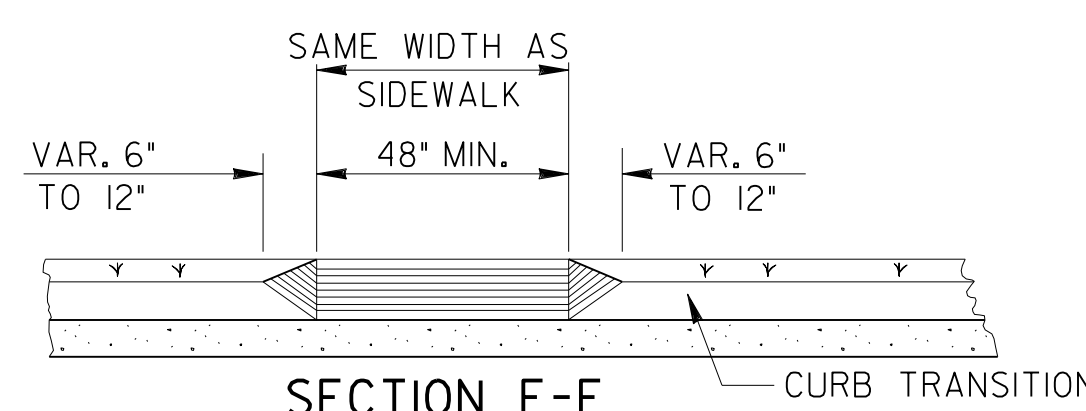
### Skewed Ramp Details

(Applies to Type A Type D Ramps Only)

WHEN THE RAMP CENTERLINE IS NOT PERPENDICULAR TO THE CURB A LEVEL LANDING AREA WITH SLOPES LESS THAN 2% MUST BE PROVIDED AT THE BOTTOM OF THE RAMP.



### PLAN VIEW



### This Detail Replaces Ga Standard 9031W

Guidelines For Usage On Metric Projects

When these details are incorporated into plans and or projects that are being prepared or constructed in metric units, exact or precise conversion to metric units is not required. The dimensions shown that are in feet and inches may be converted to corresponding metric units using the following "Rounded-Off" conversion factors: 1" = 25mm, 4" = 100mm, and 12" or 1' = 300mm. All measurement notes that refer to linear feet and square yards shall be interpreted to mean linear meters and square meters.

| DEPARTMENT OF TRANSPORTATION<br>STATE OF GEORGIA                           |          |         |                |  |  |  |  |  |  |
|--|----------|---------|----------------|--|--|--|--|--|--|
| SPECIAL DETAIL<br>CONCRETE SIDEWALK DETAILS<br>CURB CUT (WHEELCHAIR) RAMPS |          |         |                |  |  |  |  |  |  |
| NO   | REVISION | DATE    |                |  |  |  |  |  |  |
| 9-15-16  | REVISED  | 5-10-06 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 2-21-03 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 2-10-03 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 7-29-02 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 5-29-02 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 5-23-02 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 5-13-02 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 4-29-02 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 4-11-02 |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 4-3-02  |                |  |  |  |  |  |  |
| 6-18-09  | REVISED  | 3-28-02 |                |  |  |  |  |  |  |
| NO SCALE   |          |         | MARCH 12, 2002 |  |  |  |  |  |  |
|  |          |         | NUMBER<br>A3   |  |  |  |  |  |  |